

Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

NORTH BASSETT STREET, CONKLIN-UNIVERSITY ALLEY, AND HENRY-BROOM ALLEY ASSESSMENT DISTRICT - 2017

CITY PROJECT NO. 11430
CONTRACT NO. 7847

PUBLIC IMPROVEMENT PROJECT APPROVED

APRIL 18, 2017

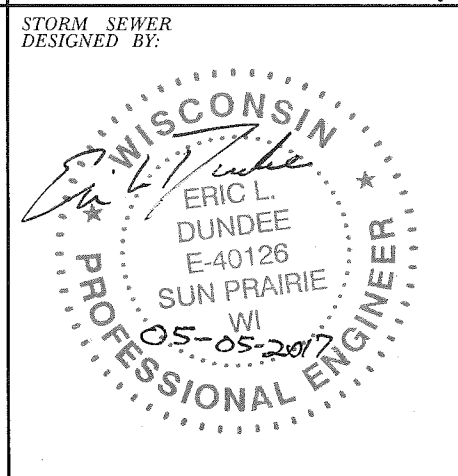
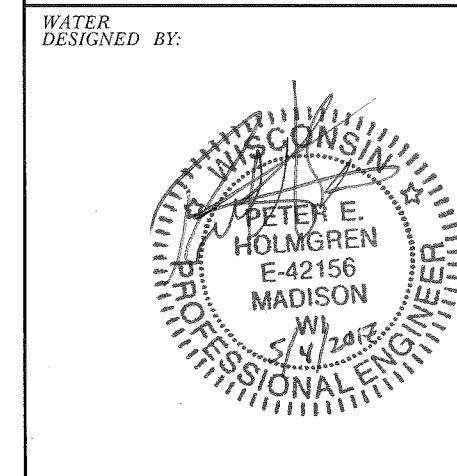
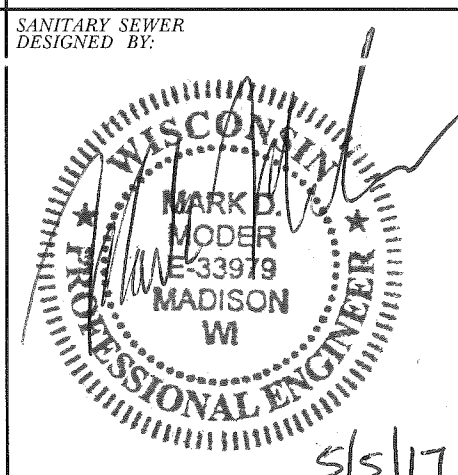
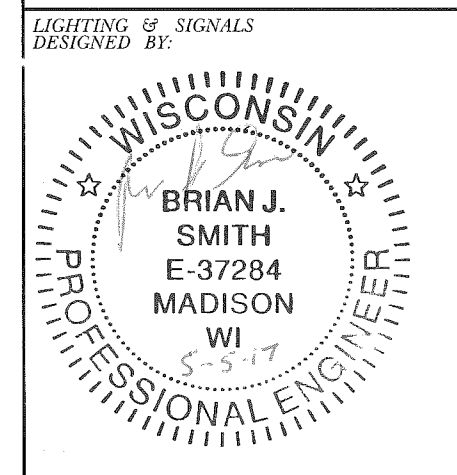
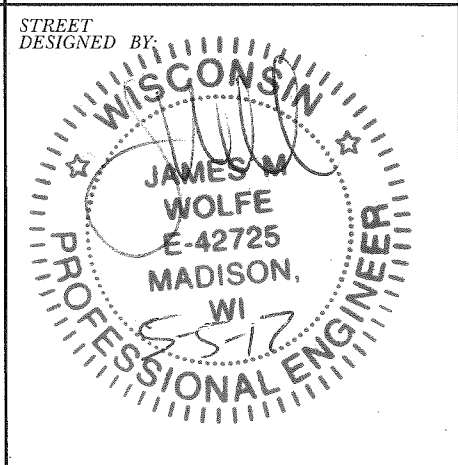
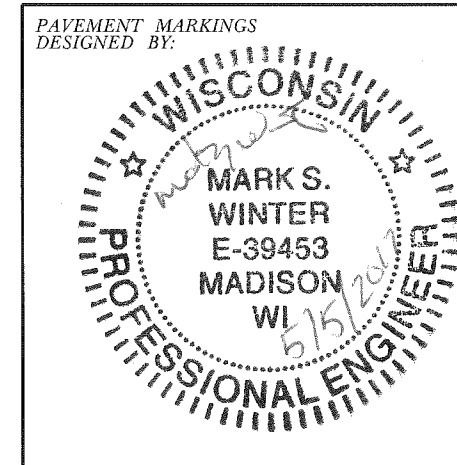
BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

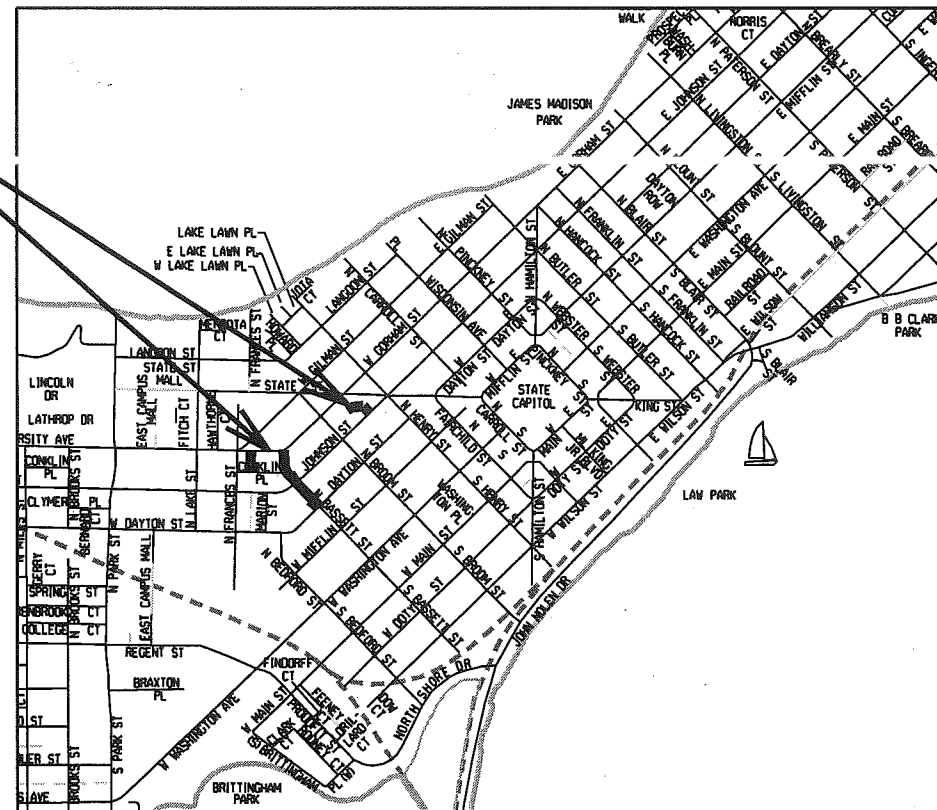
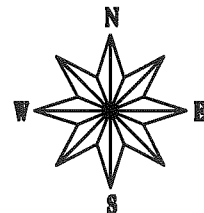
[Signature] 5/8/17
City Engineer Date

INDEX OF SHEETS

SHEET NO.	TITLE
1	DETAILS
DI-D7	EROSION CONTROL PLANS
EC1-EC2	STREET PLAN & PROFILES
PI-P4	SEWERS PLAN & PROFILES
U1-U3	SEWER SCHEDULES
U4-U6	WATER PLAN & PROFILES
W1-W2	WATER IMPACT PLAN & MATERIALS
W3-W4	PAVEMENT MARKING PLAN
M1-M2	ELECTRICAL PLAN
E1-E9	TRAFFIC CONTROL PLAN
TC1-TC15	CROSS SECTIONS
X1-X9	



PROJECT
LOCATION



CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS	— G —
STORM SEWER	— ST —
SANITARY SEWER	— SAN —
WATER	— W —
OVERHEAD ELECTRIC	— OH —
BURIED ELECTRIC	— E —
LIGHT POLE	☼

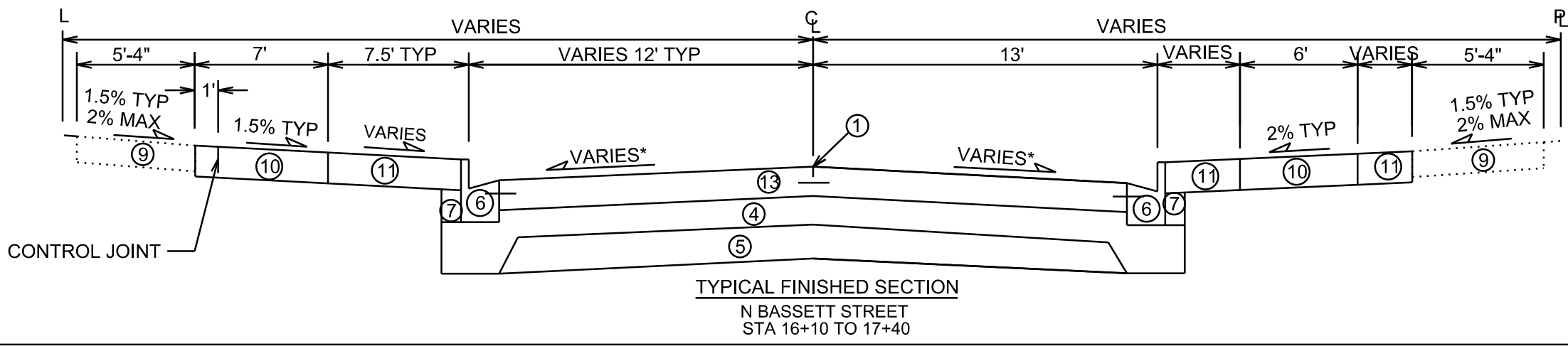
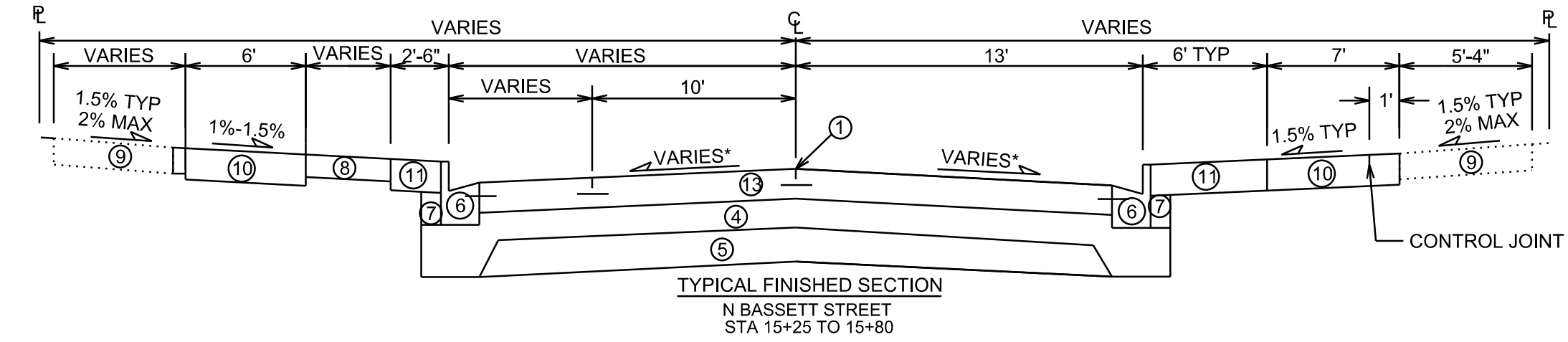
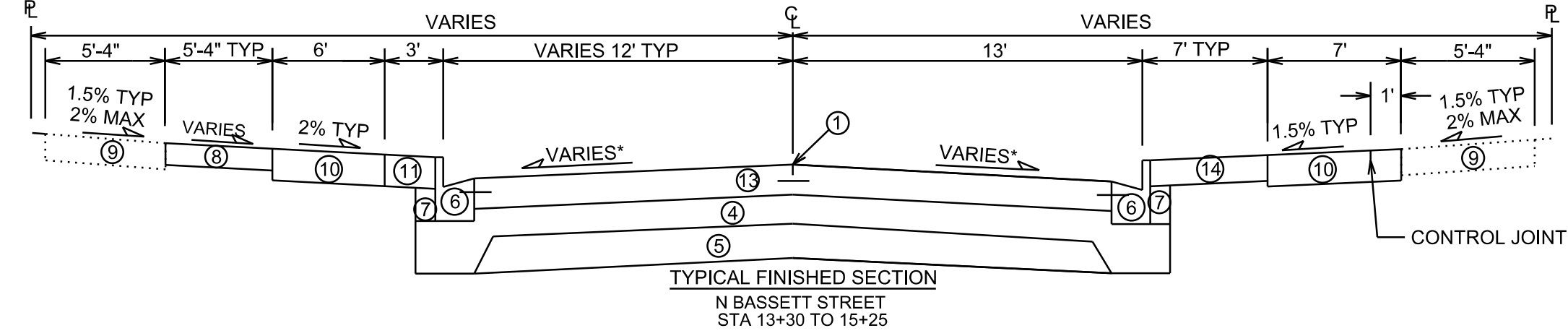
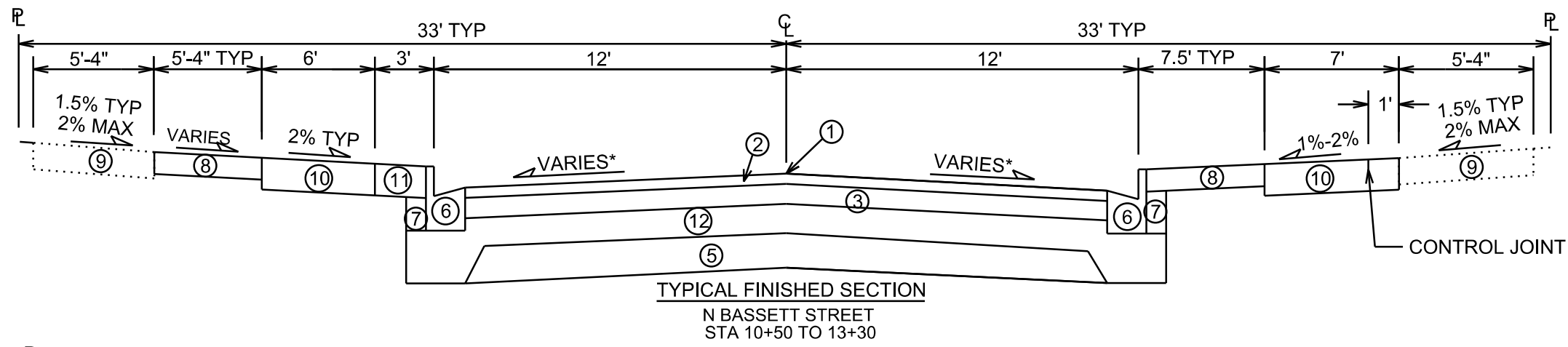
EARTH WORK SUMMARY:
EXCAVATION CUT (MEASURED PLAN QUANTITY).....1,350 C.Y.
ESTIMATED UNDISTRIBUTED UNDERCUT.....260 C.Y.
TOTAL UNCLASSIFIED EXCAVATION CUT.....1,610 C.Y.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



- ① POINT REFERRED TO ON PROFILE
- ② 1.75" BITUMINOUS UPPER LAYER, TYPE E-3
- ③ 3.5" BITUMINOUS LOWER LAYER, TYPE E-3
- ④ 4" GRADATION 2 CRUSHED STONE
- ⑤ 6" GRADATION 1 CRUSHED STONE
- ⑥ TYPE 'A' CONCRETE CURB & GUTTER
- ⑦ FILL, INCIDENTAL
- ⑧ 6" TOPSOIL, SOD
- ⑨ 5" CONCRETE SIDEWALK, REPLACE AS NOTED ON PLAN
- ⑩ 7" COLORED CONCRETE BIKE LANE**
- ⑪ 7" CONCRETE TERRACE
- ⑫ 6" GRADATION 2 CRUSHED STONE
- ⑬ 9" CONCRETE PAVEMENT***

TYPICAL SECTIONS NOT TO SCALE

* SEE CROSS SECTION SHEETS FOR CROSS SLOPES AND TOP OF CURB ELEVATIONS.

** TRANSVERSE JOINTS FOR BIKE LANE SHALL BE SAWED, AND SPACE APPROXIMATELY 8 FT. JOINTS FOR 1 FT. BUFFER ADJACENT TO SIDEWALK SHALL BE SPACED AT APPROXIMATELY 2 FT. INSTALL BOND BREAKER ON EITHER SIDE OF 1 FT. BUFFER.

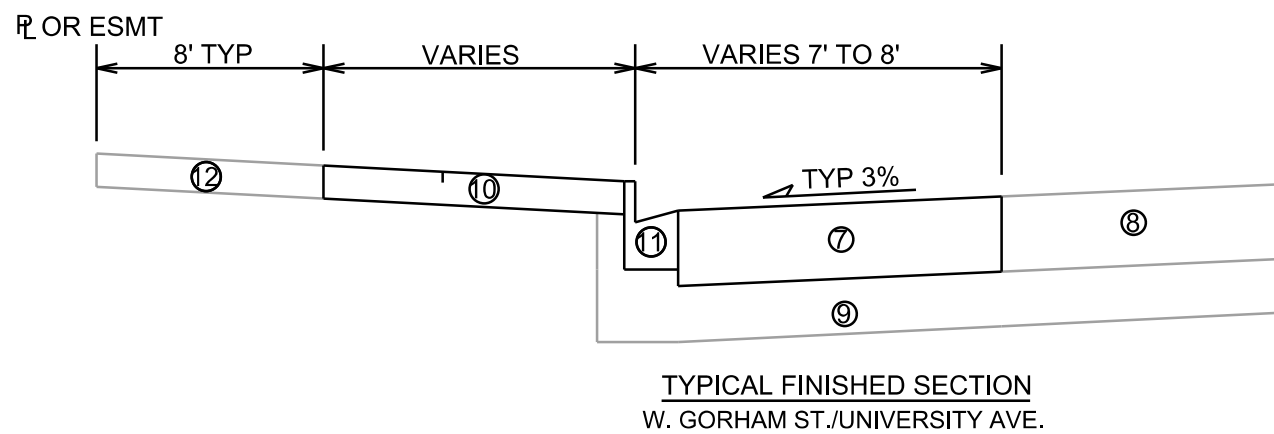
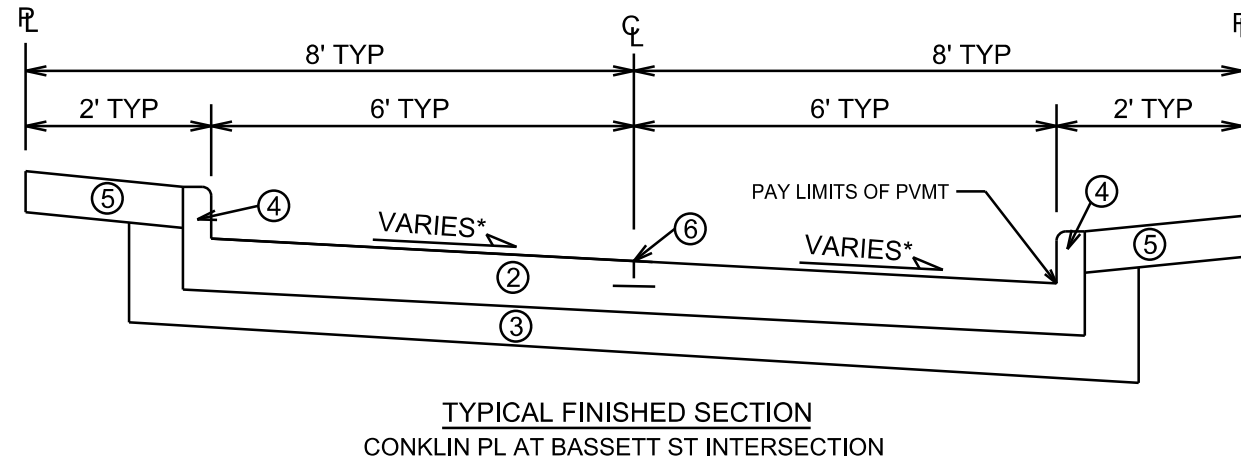
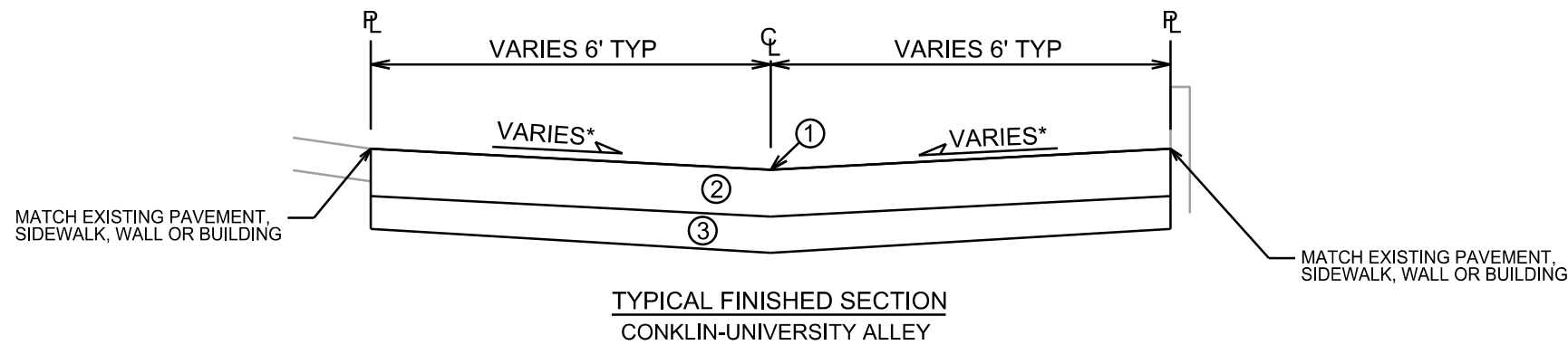
*** INSTALL TIE & DOWEL BARS PER STANDARD SPECIFICATIONS & DETAILS. TRANSVERSE JOINTS SPACED AT APPROXIMATELY 15 FT.

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



- ① POINT REFERRED TO ON PROFILE
- ② 8" CONCRETE PAVEMENT**
- ③ 8" GRADATION 2 CRUSHED STONE
- ④ 6" INTEGRAL CONCRETE CURB***
- ⑤ 7" CONCRETE SIDEWALK
- ⑥ LONGITUDNAL JOINT PER STND SPECS
- ⑦ 10" CONCRETE PAVEMENT
- ⑧ EXISTING 10" CONCRETE PAVEMENT
- ⑨ EXISTING BASE, REGRADE & COMPACT
- ⑩ 5" CONCRETE SIDEWALK
- ⑪ TYPE 'A' CONC CURB & GUTTER WITH 4" CURB HEAD; PAY AS TYPE 'A'
- ⑫ EXISTING CONC SIDEWALK

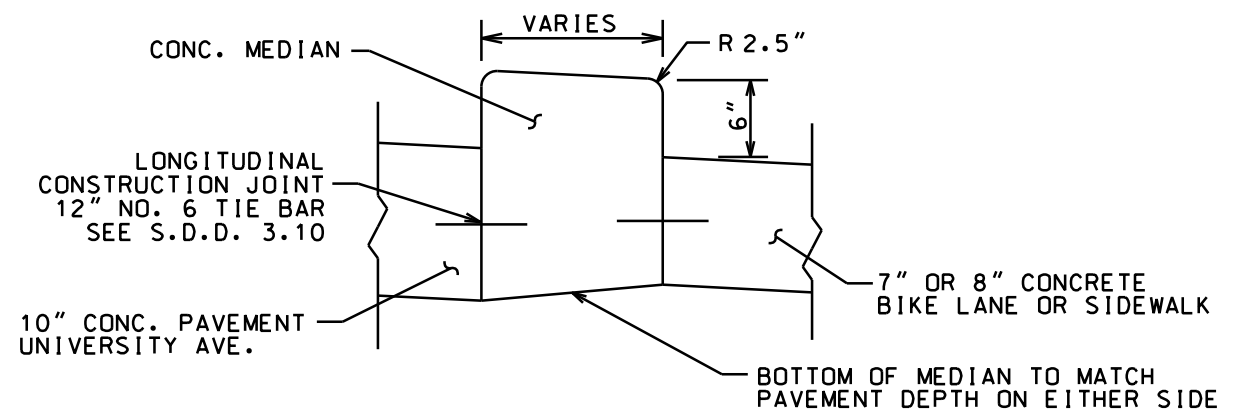
TYPICAL SECTIONS NOT TO SCALE

* SEE CROSS SECTION SHEETS FOR CROSS SLOPES AND TOP OF CURB ELEVATIONS.

** CONKLIN-UNIVERSITY ALLEY TO BE CONSTRUCTED WITHOUT A JOINT ALONG THE CENTERLINE. TYPICAL TRANSVERSE JOINT SPACING IS APPROX. 13'

*** INTEGRAL CONCRETE CURB GEOMETRY SHALL MATCH TYPE 'A' AND IS PAID SEPARATELY UNDER APPROPRIATE ITEM

CONCRETE MEDIAN DETAIL



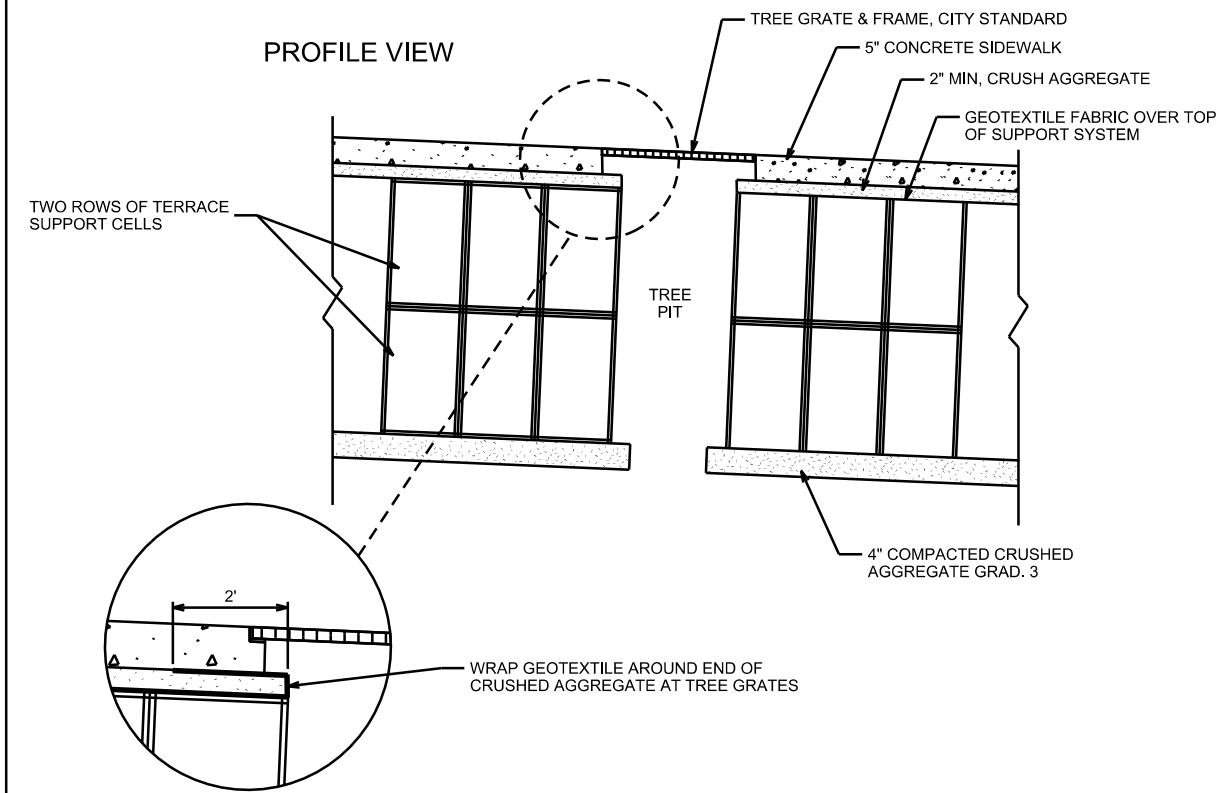
PLOT SCALE: _____

PLOT NAME: _____

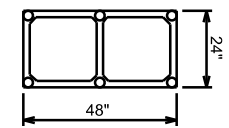
REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

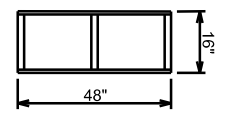
TERRACE SUPPORT SYSTEM DETAILS



TOP/BOTTOM OF FRAME

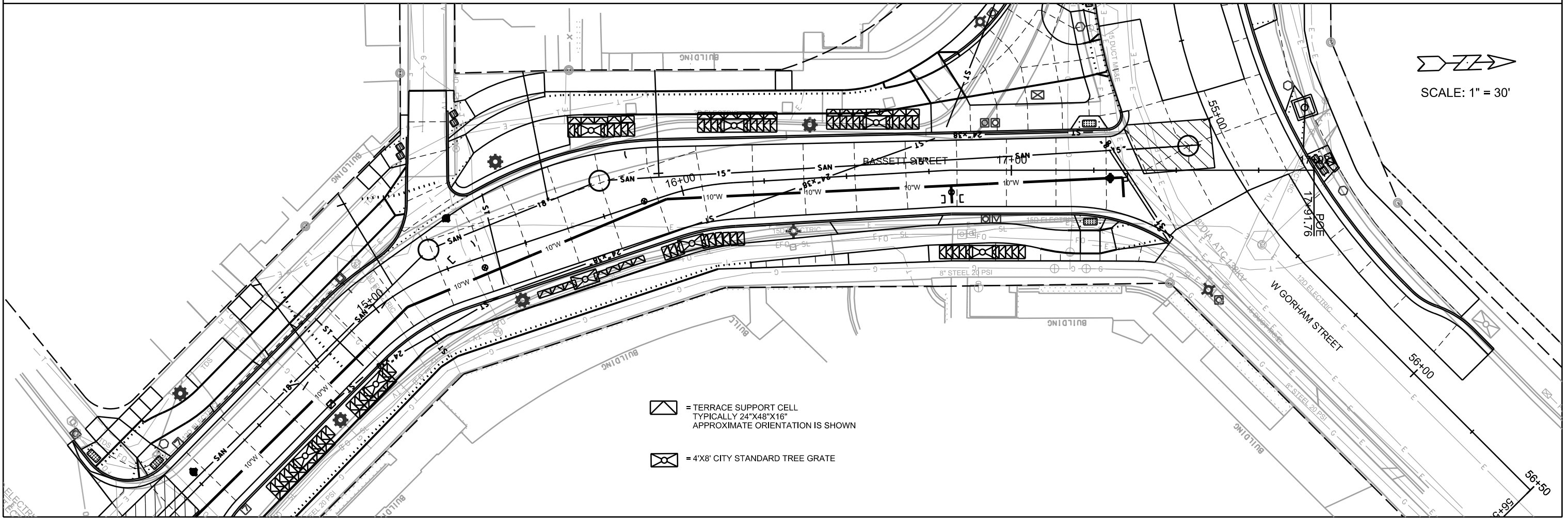


SIDE OF FRAME



NOTES:

1. COORDINATE WITH CITY FORESTRY ON FINAL TREE LOCATIONS AND PLANTING.
2. INSTALL TERRACE SUPPORT SYSTEM PER MANUFACTURER SPECIFICATIONS AND PER THE DETAILS PROVIDED.
3. ALL PERIMETERS OF THE SUSPENDED PAVEMENT SYSTEM SHALL BE ENCLOSED WITH A GEOGRID AS REQUIRED BY THE MANUFACTURER'S SPECIFICATIONS.
4. A MINIMUM 6' GAP, CENTERED ON THE TREE GRATE OPENING, SHALL BE PROVIDED FOR TREE PLANTING.
5. BACKFILL THE SUSPENDED PAVEMENT SYSTEM WITH TOPSOIL (PAID AS TOPSOIL BACKFILL) AND BACKFILL SHALL REMAIN MINIMALLY COMPACTED. LARGER VOIDS WHERE CELLS CANNOT BE INSTALLED SHALL BE BACKFILLED WITH 1" CLEAR STONE (PAID SEPARATELY) AND WRAPPED WITH GEOTEXTILE FABRIC.
6. TERRACE SUPPORT SYSTEM SHALL BE A MINIMUM OF 1 FT. OFF THE BACK OF CURB, IN ALL LOCATIONS.
7. IN LOCATIONS WHERE THE TERRACE SUPPORT SYSTEM NEEDS TO BE GAPPED AROUND UTILITIES, BACKFILL GAPPED AREAS AROUND UTILITIES WITH 1" CLEAR STONE, AND WRAP WITH GEOTEXTILE FABRIC.
8. INSTALL CONCRETE SIDEWALK & TREE GRATES PER CITY STANDARD SPECS. IN LOCATIONS WHERE TREE GRATE IS SUPPORTED BY LESS THAN 1' OF CONCRETE SIDEWALK BETWEEN CURB OR JOINT, INSTALL 10' #4 REBAR ALONG LENGTH OF GRATE, WHICH SHALL BE CONSIDERED INCIDENTAL TO TREE GRATE.



PLOT SCALE:

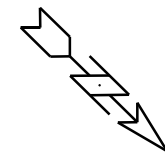
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SCALE: 1" = 30'

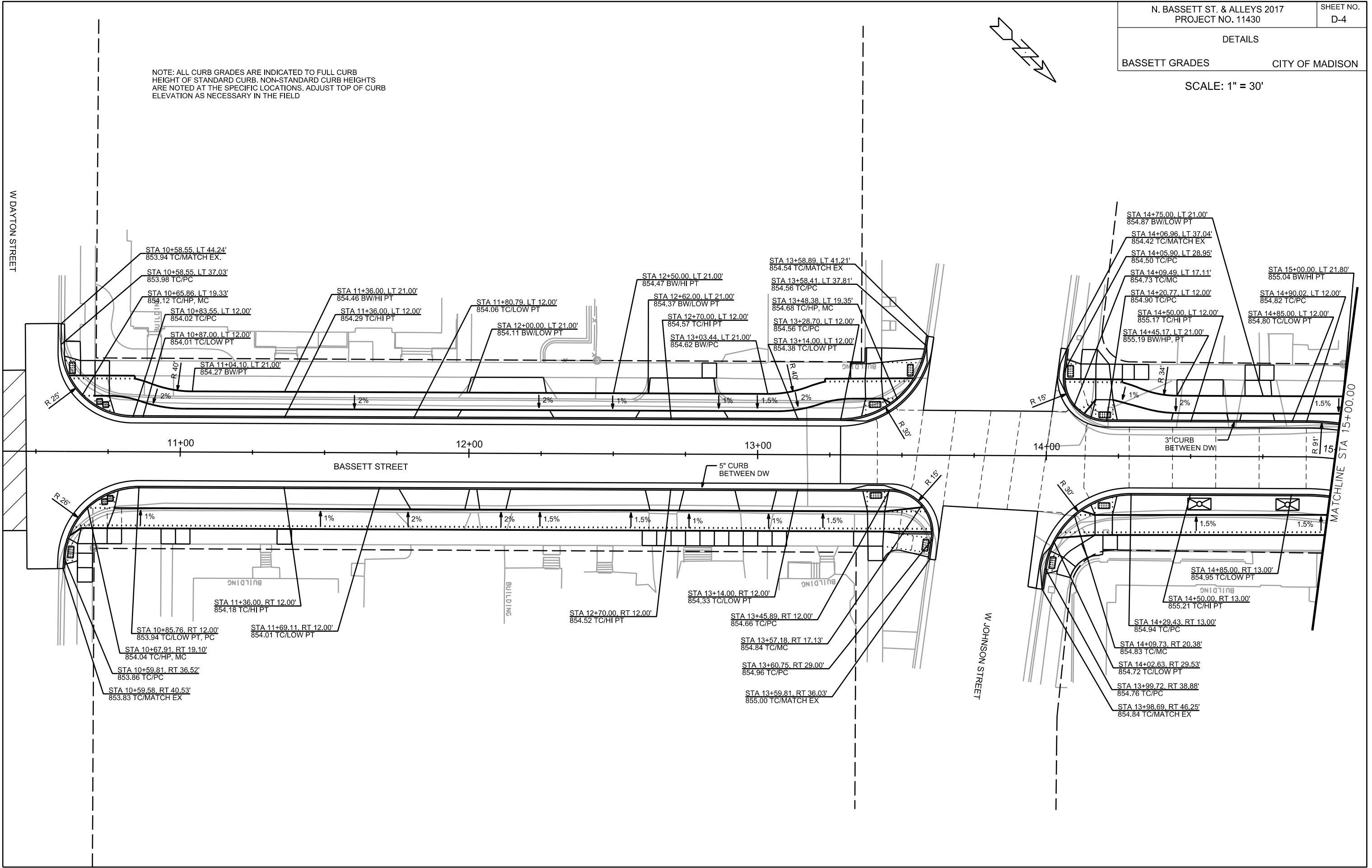
NOTE: ALL CURB GRADES ARE INDICATED TO FULL CURB HEIGHT OF STANDARD CURB. NON-STANDARD CURB HEIGHTS ARE NOTED AT THE SPECIFIC LOCATIONS. ADJUST TOP OF CURB ELEVATION AS NECESSARY IN THE FIELD



PLOT SCALE: _____
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



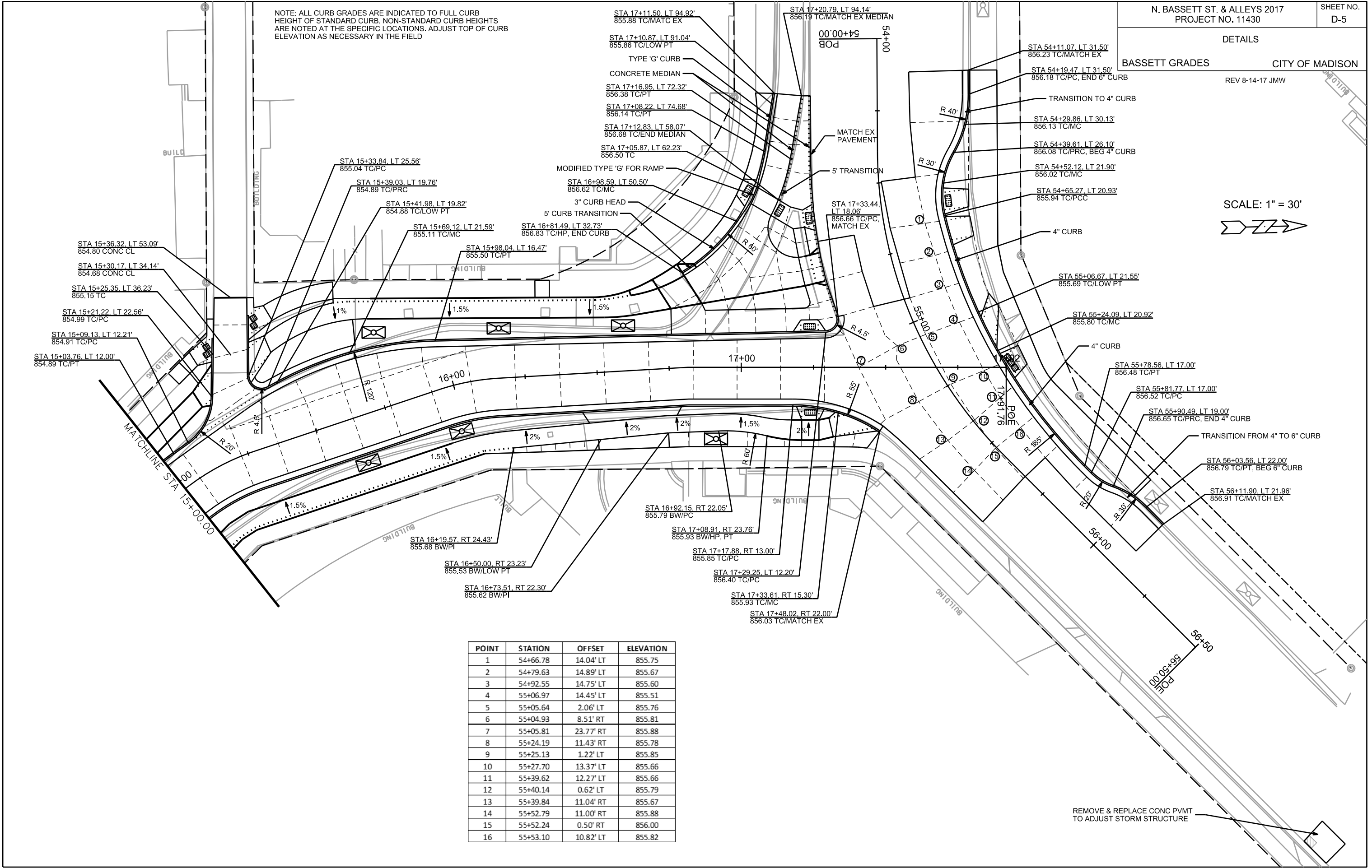
DETAILS
BASSETT GRADES CITY OF MADISON

REV 8-14-17 JMW

SCALE: 1" = 30'



NOTE: ALL CURB GRADES ARE INDICATED TO FULL CURB HEIGHT OF STANDARD CURB. NON-STANDARD CURB HEIGHTS ARE NOTED AT THE SPECIFIC LOCATIONS. ADJUST TOP OF CURB ELEVATION AS NECESSARY IN THE FIELD



POINT	STATION	OFFSET	ELEVATION
1	54+66.78	14.04' LT	855.75
2	54+79.63	14.89' LT	855.67
3	54+92.55	14.75' LT	855.60
4	55+06.97	14.45' LT	855.51
5	55+05.64	2.06' LT	855.76
6	55+04.93	8.51' RT	855.81
7	55+05.81	23.77' RT	855.88
8	55+24.19	11.43' RT	855.78
9	55+25.13	1.22' LT	855.85
10	55+27.70	13.37' LT	855.66
11	55+39.62	12.27' LT	855.66
12	55+40.14	0.62' LT	855.79
13	55+39.84	11.04' RT	855.67
14	55+52.79	11.00' RT	855.88
15	55+52.24	0.50' RT	856.00
16	55+53.10	10.82' LT	855.82

REMOVE & REPLACE CONC PVMT TO ADJUST STORM STRUCTURE

PLOT SCALE:

PLOT NAME:

REV. DATE:

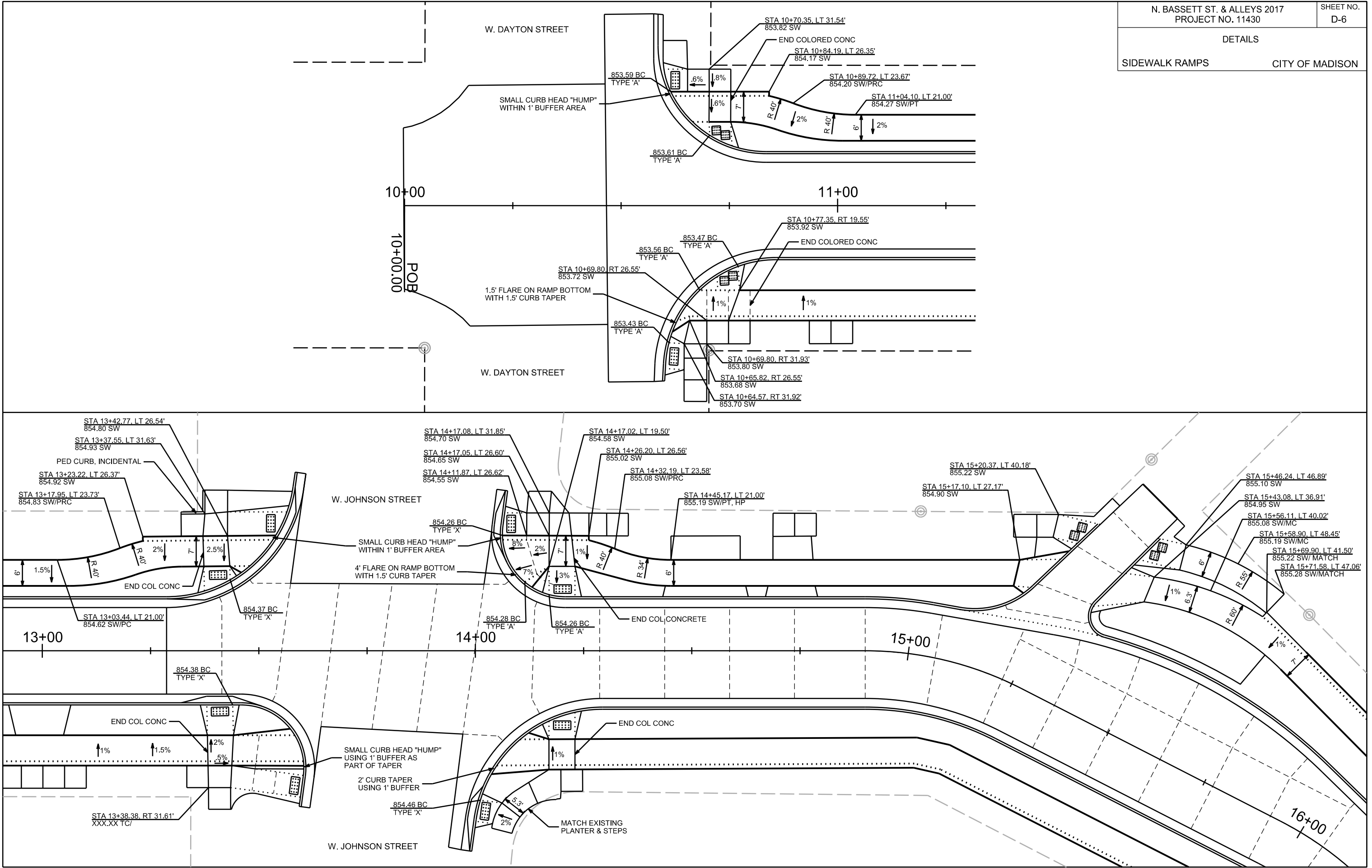
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

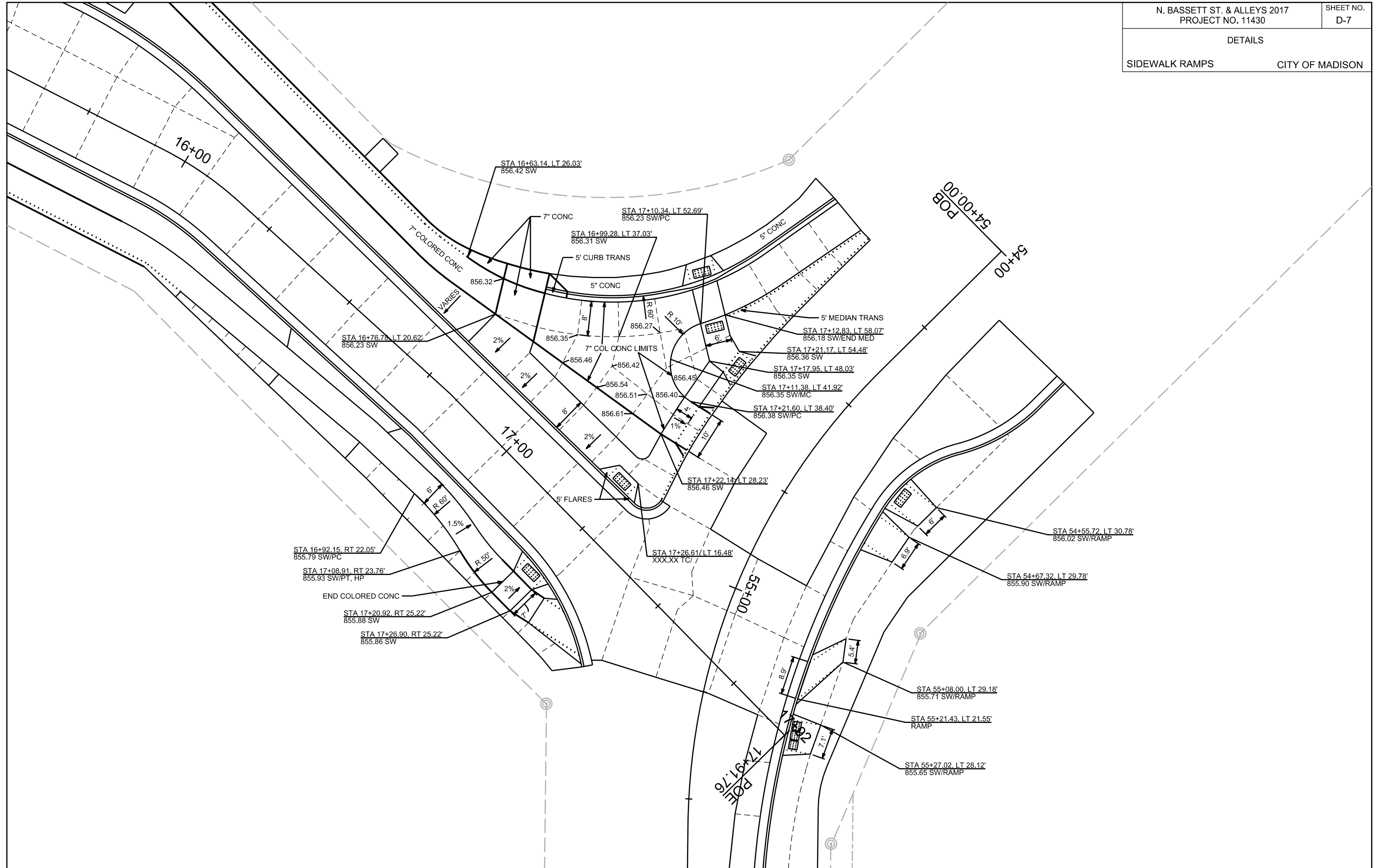
PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION





PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

EROSION CONTROL PLAN

N. BASSETT ST. CITY OF MADISON

0709-232-1131-5
LUETKE TRUST, CHARLES W
ATTN FAIRCREST MNGMT
308 N Bassett St

0709-232-1128-2
BURNISTER JT REV TRUST
% MADISON PROPERTY MNGMT
504 W Johnson St

0709-231-1503-8
FAUST SCOTT
ATTN BROADWALK INV LLC
222 N Bassett St

0709-231-1502-0
KLAR PROPERTIES LLP
220 N Bassett St

0709-231-1529-4
PARK PLACE PROPERTIES LLC
ATTN BOARDWALK INV LLC
210 N Bassett St

0709-231-2406-3
MADISON COMMUNITY COOP
122 N Bassett St

0709-231-1320-6
VAN ROOY, CARL J & JANET
% WI MANAGEMENT CO INC
123 N Bassett St

0709-231-1421-2
LEXINGTON APTS
ATTN H & R MANAGEMENT
454 W Dayton St

0709-231-1420-4
215 N BASSETT LLC
215 N Bassett St

0709-231-1419-7
MANALO, FELIPE & MARIA
217 N Bassett St

0709-231-1418-9
BASSETT STREET LLC
C/O STEVEN J HELLENBRAND
219 N Bassett St

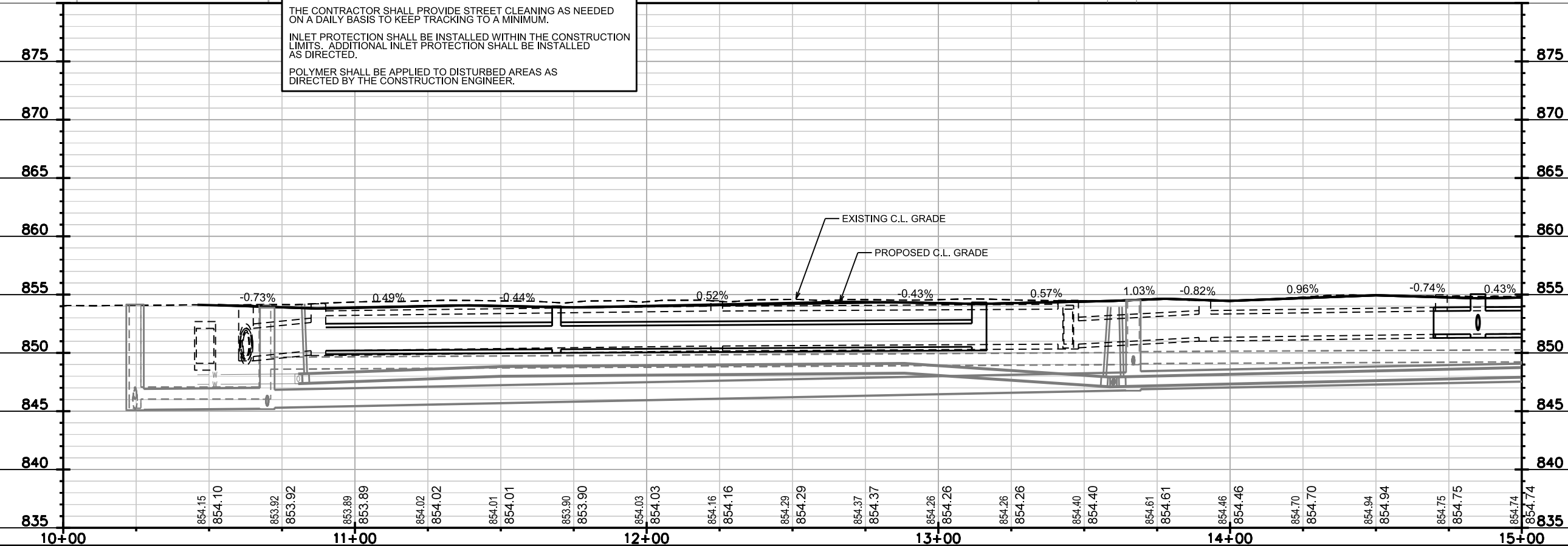
0709-231-1417-1
LOFGREEN PROPERTIES 8 LLC
221 N Bassett St

0709-231-0530-2
BASSETT JOHNSON LLC
440 W Johnson St

EROSION CONTROL NOTES:
EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY OTHER CONSTRUCTION ACTIVITY.
THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION AND MAINTENANCE OF ALL EROSION CONTROL MEASURES UNTIL FINAL ACCEPTANCE BY THE CITY OF MADISON.
THE CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE WDNR SOC STANDARDS AND CITY OF MADISON STANDARD SPECIFICATIONS.
THE CONTRACTOR SHALL PROVIDE STREET CLEANING AS NEEDED ON A DAILY BASIS TO KEEP TRACKING TO A MINIMUM.
INLET PROTECTION SHALL BE INSTALLED WITHIN THE CONSTRUCTION LIMITS. ADDITIONAL INLET PROTECTION SHALL BE INSTALLED AS DIRECTED.
POLYMER SHALL BE APPLIED TO DISTURBED AREAS AS DIRECTED BY THE CONSTRUCTION ENGINEER.

	CONSTRUCTION ENTRANCE
	TYPE D HYBRID INLET PROTECTION
	TYPE C INLET PROTECTION

MATCHLINE STA 15+00.00

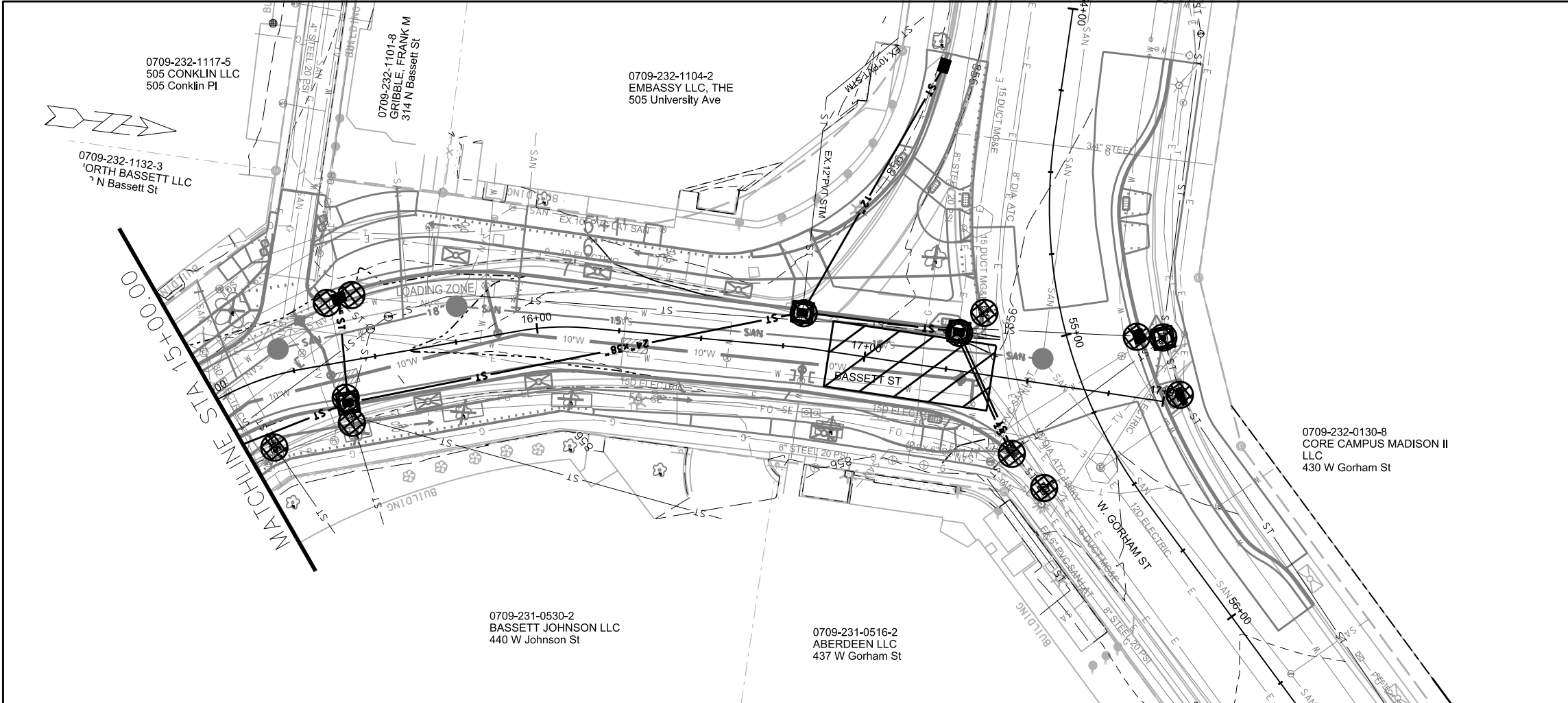


PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CONSTRUCTION ENTRANCE

TYPE D HYBRID INLET PROTECTION

TYPE C INLET PROTECTION

EROSION CONTROL NOTES:

EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY OTHER CONSTRUCTION ACTIVITY.

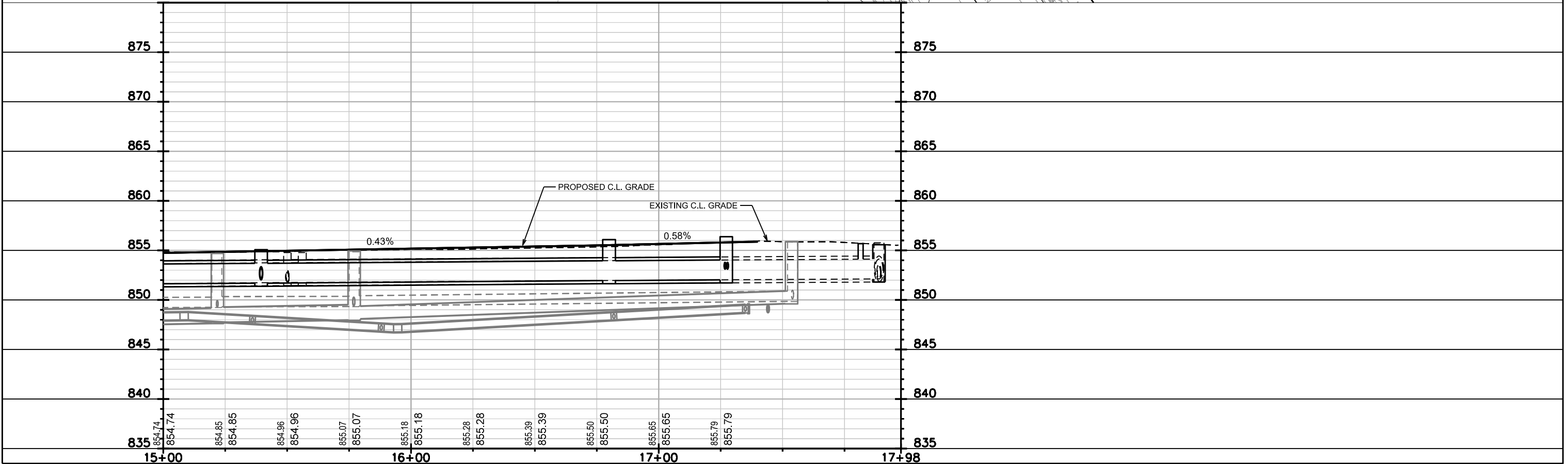
THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION AND MAINTENANCE OF ALL EROSION CONTROL MEASURES UNTIL FINAL ACCEPTANCE BY THE CITY OF MADISON.

THE CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE WDNR SOC STANDARDS AND CITY OF MADISON STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL PROVIDE STREET CLEANING AS NEEDED ON A DAILY BASIS TO KEEP TRACKING TO A MINIMUM.

INLET PROTECTION SHALL BE INSTALLED WITHIN THE CONSTRUCTION LIMITS. ADDITIONAL INLET PROTECTION SHALL BE INSTALLED AS DIRECTED.

POLYMER SHALL BE APPLIED TO DISTURBED AREAS AS DIRECTED BY THE CONSTRUCTION ENGINEER.



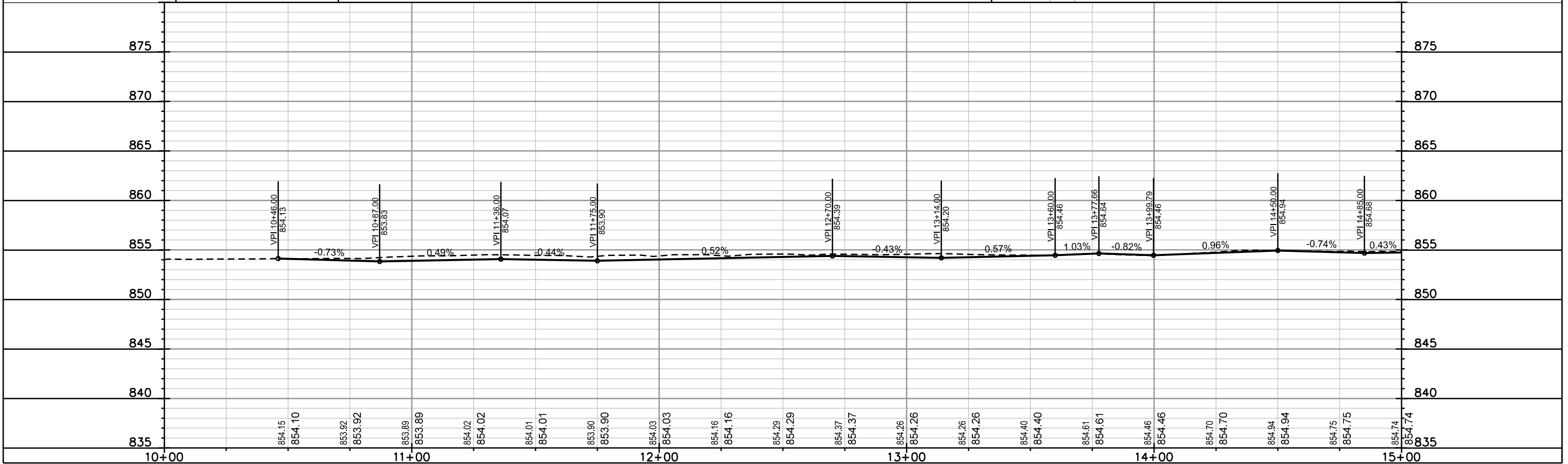
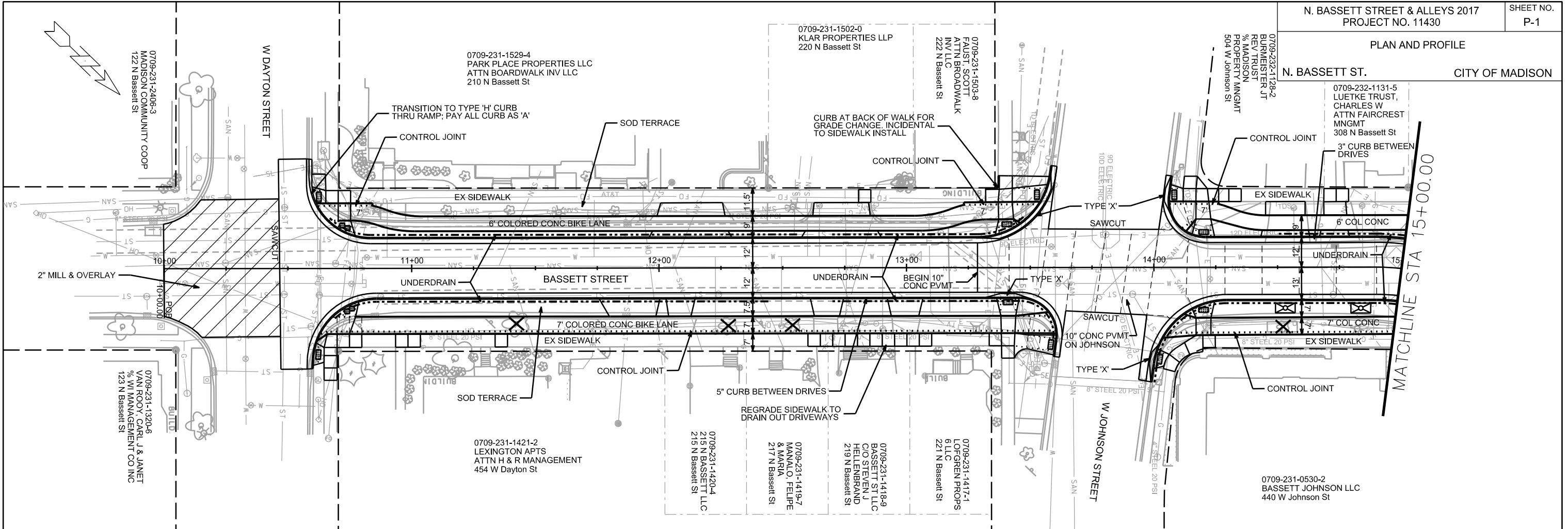
PLOT SCALE:

PLOT NAME:

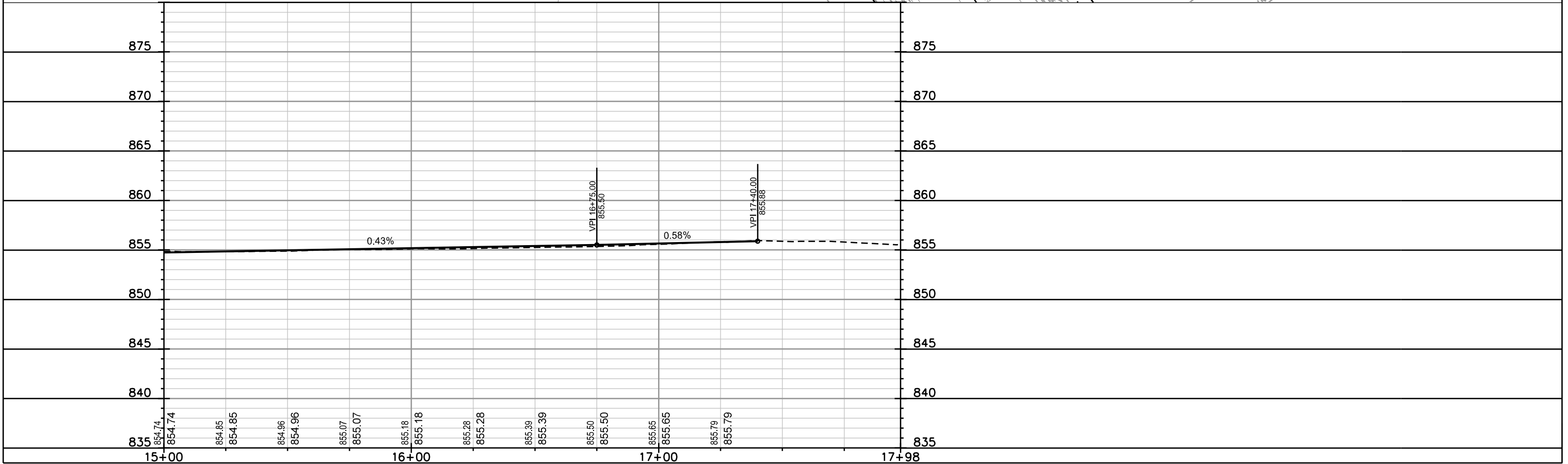
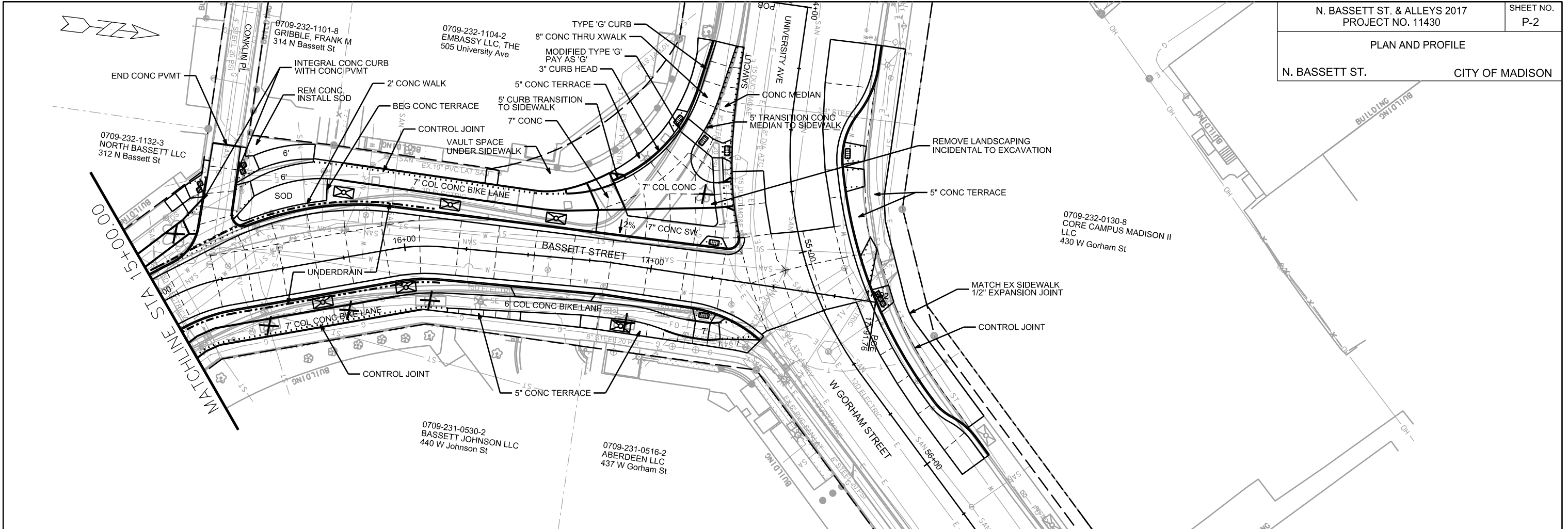
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLAN AND PROFILE
N. BASSETT ST. CITY OF MADISON



PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION



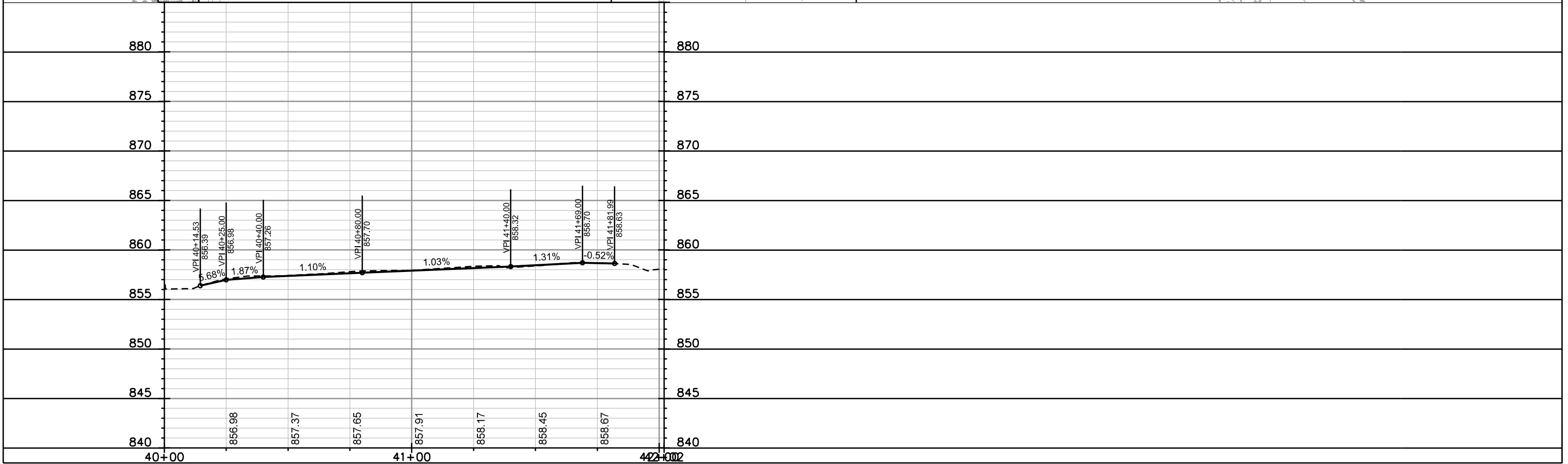
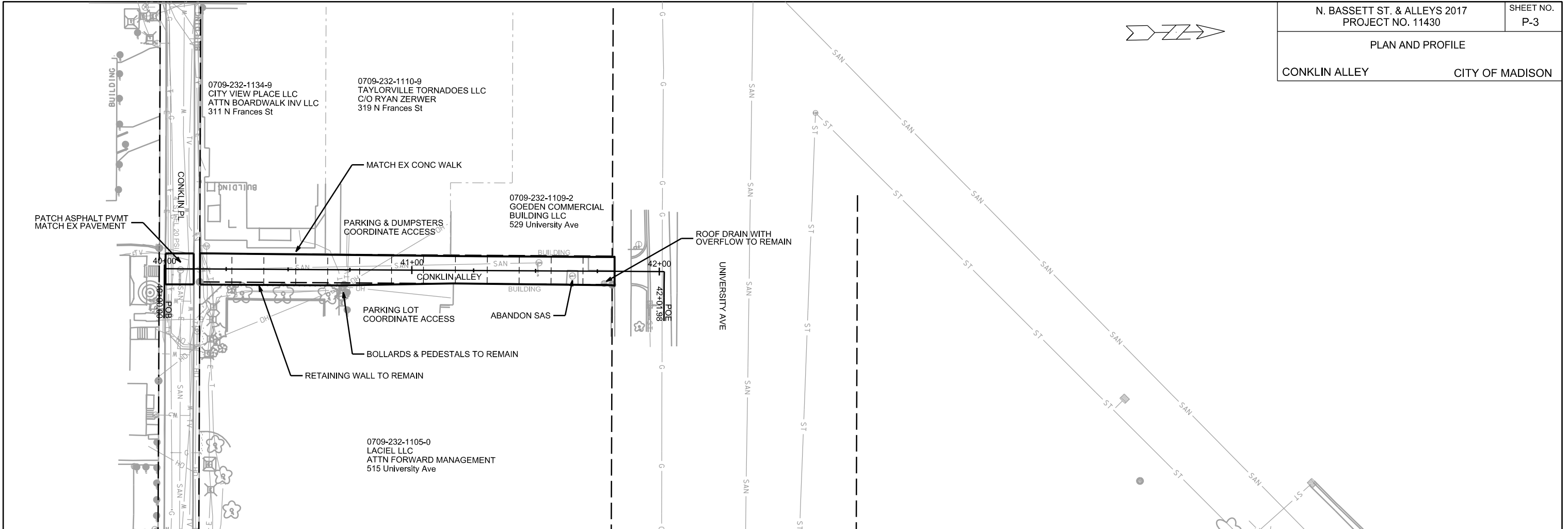
PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLAN AND PROFILE
CONKLIN ALLEY CITY OF MADISON



PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

0709-231-0308-3
317 WEST GORHAM ST LLC
315 W Gorham St

0709-231-0307-5
MCCAUGHIEY DEVELOPMENT
ASSOCIATES
341 State St

EXISTING EGRESS DOORS; GRADE
PAVEMENT TO DRAIN AWAY & MAINTAIN
DOOR ACCESS (DOORS OPEN INTO ALLEY)

BOLLARDS & ROOF DRAINS TO REMAIN

0709-231-0306-7
GSV PROPERTIES LLC
327 State St




0709-231-0305-9
BAUER, CHARLES T
CHARLES E BECKWITH
319 State St

0709-231-0304-1
MACKESY TRUST, MARK
317 State St

0709-231-0303-3
MITROPOULOS
FAM REV TRUST
311 State St

0709-231-0312-4
FIRE HOUSE JT VENT PRSHP
% NATHAN F BRAND
301 N BROOM ST

0709-231-0314-0
322 WEST JOHNSON ST LLP
322 W Johnson St

-  = ALLEY CONCRETE PAVEMENT REPAIR
NOTE: SEAL CONCRETE PAVEMENT JOINTS & ANY CRACKS THAT REMAIN.
JOINT/CRACK SEALING PAID UNDER APPROPRIATE BID ITEM
-  = PULVERIZE & SHAPE EXISTING PAVEMENT; RESURFACE ASPHALT
PAVEMENT WITH 4" TYPE E-1 PAVEMENT (2.25" LOWER, 1.75" UPPER)
-  = 4'X8' TREE GRATE; INSTALL AROUND EXISTING TREE

5" CONC TERRACE

5' CONC AROUND
POLE & SIGN

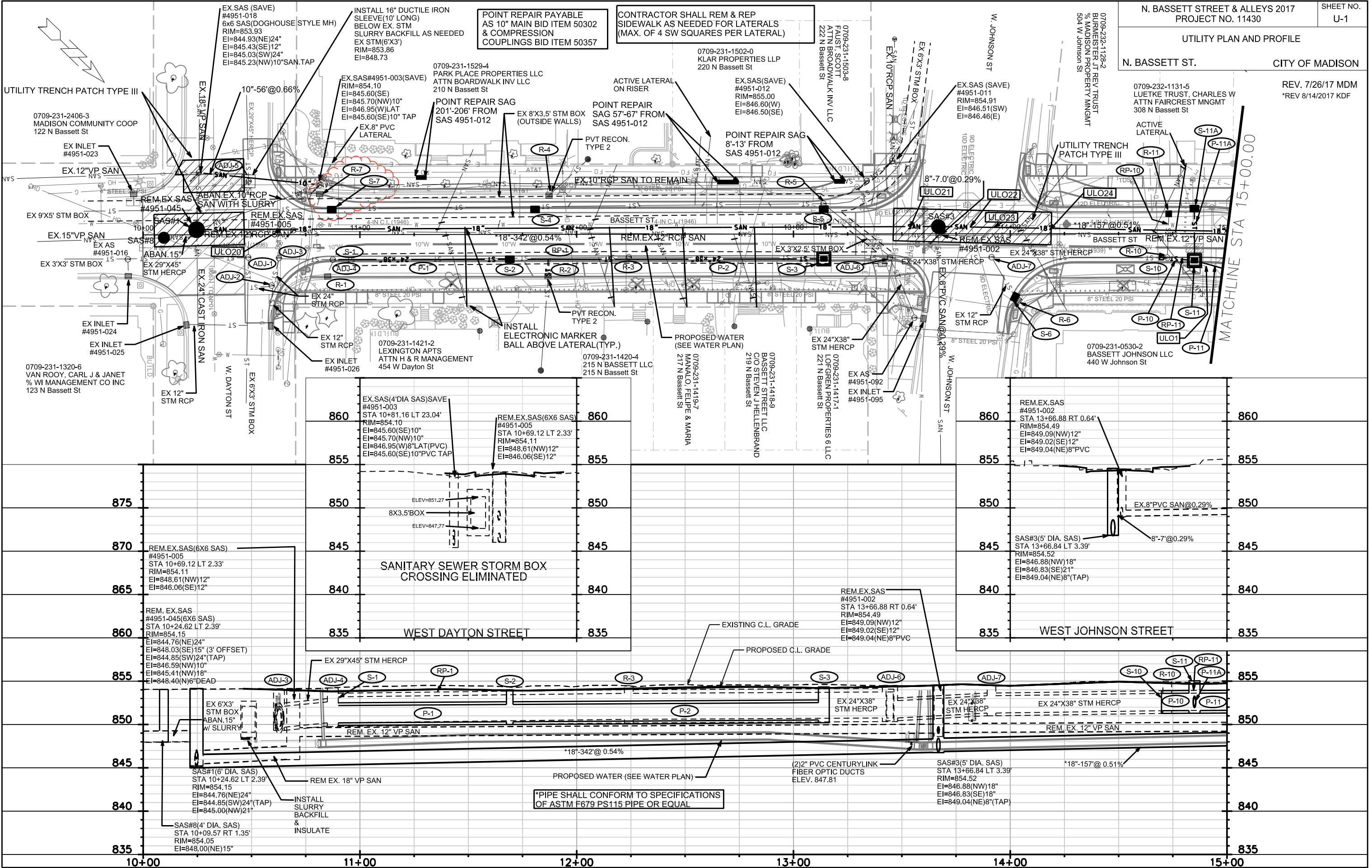
REMOVE EXISTING TERRACE & INSTALL 6" OF
TOPSOIL. TOP WITH EROSION CONTROL MAT
MINIMIZE EXCAVATION AROUND TREE

PLOT SCALE:

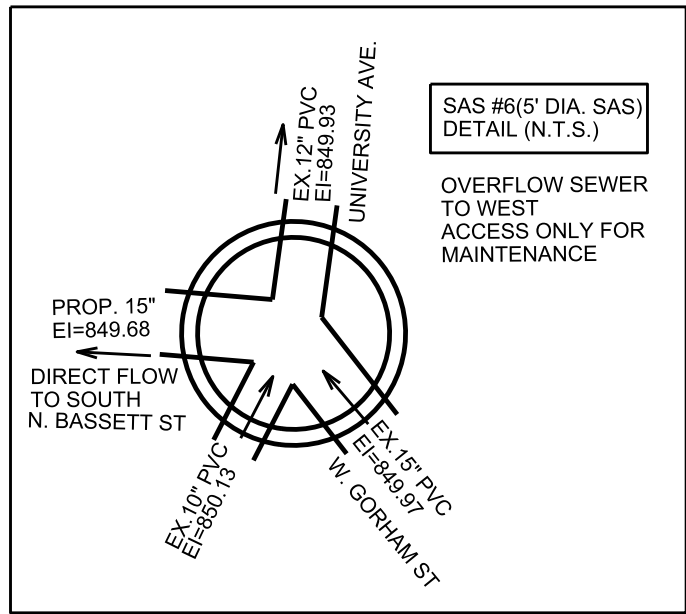
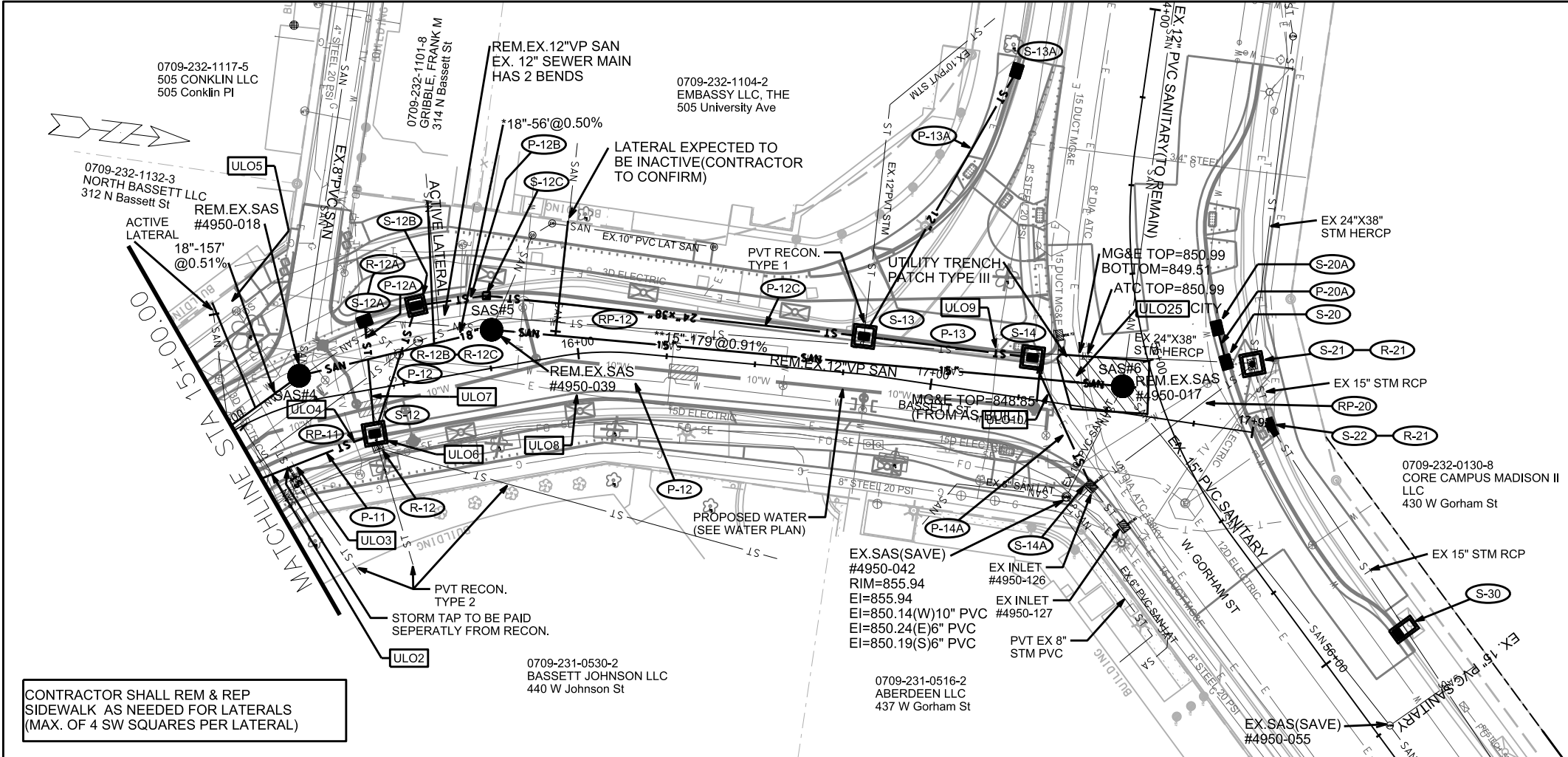
PLOT NAME:

REV. DATE:

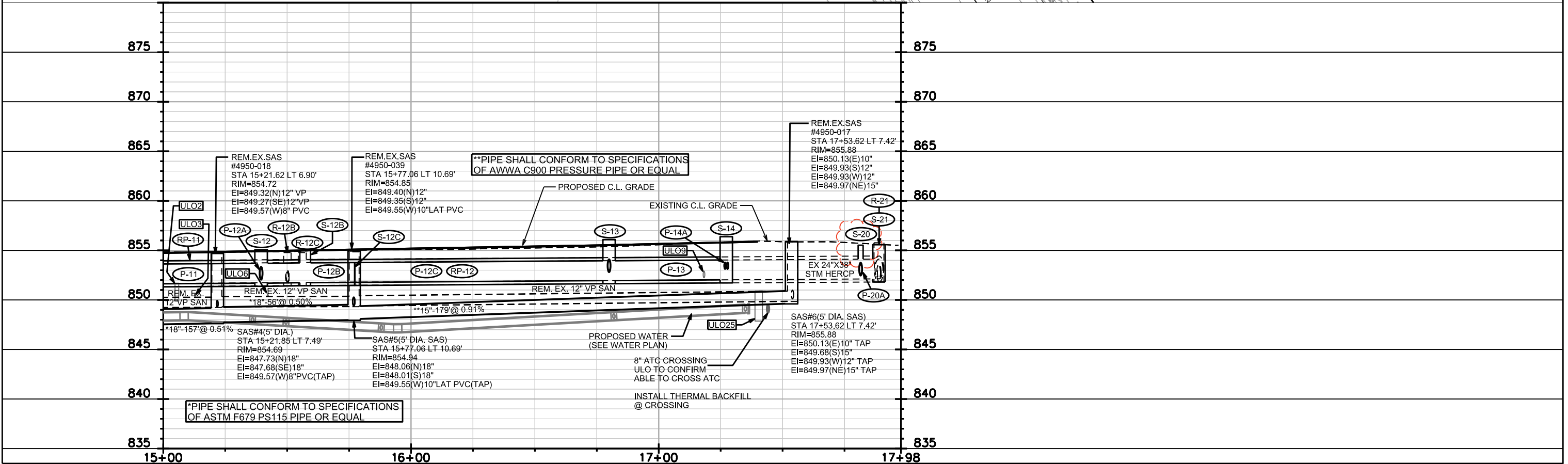
ORIGINATOR: CITY OF MADISON, STREETS DIVISION



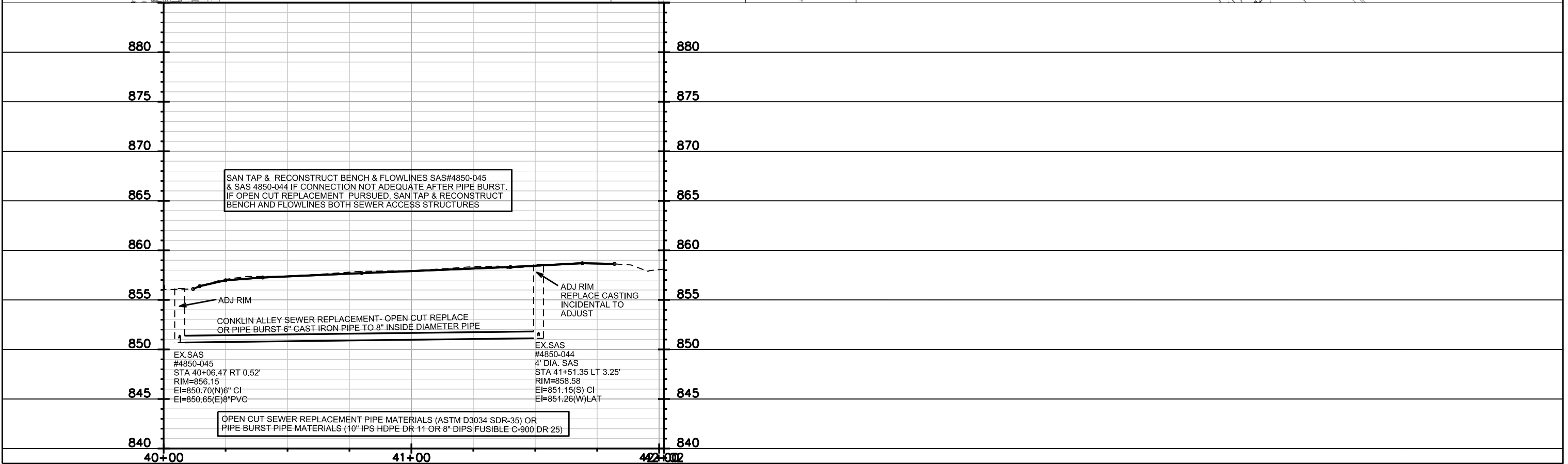
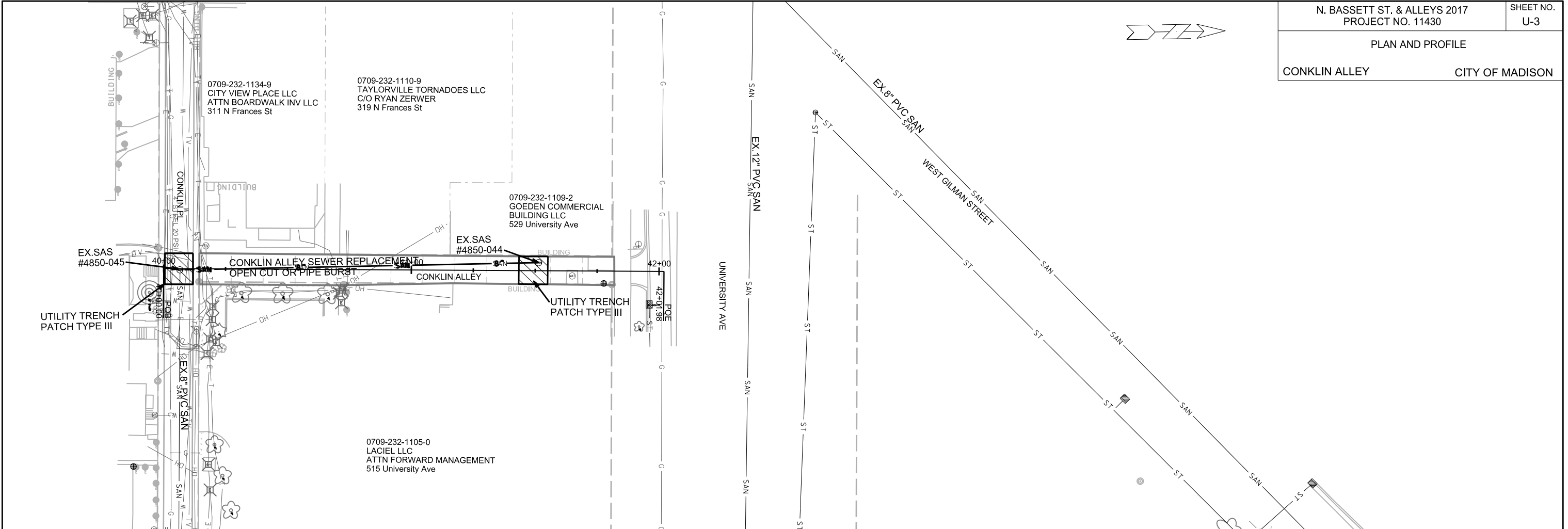
*REV 7/26/2017 KDF
*REV 8/14/2017 KDF



CONTRACTOR SHALL REM & REP
SIDEWALK AS NEEDED FOR LATERALS
(MAX. OF 4 SW SQUARES PER LATERAL)



PLOT SCALE: PLOT NAME: REV. DATE: ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SANITARY SEWER SCHEDULE

N. BASSETT STREET & ALLEYS 2017

PROJECT NO. 11430

SHEET NO.

U-4

SANITARY SEWER SCHEDULE

CITY OF MADISON

PROPOSED SANITARY STRUCTURES

SAS NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	E.I.	DEPTH	NOTES
N. BASSETT STREET						
* SAS #1	10+24.62	LT-2.39	854.15	844.76	9.39	6' DIAMENTER SAS
* SAS #2	10+69.94	LT-3.52	853.89	845.24	8.65	5' DIAMENTER SAS
* SAS #7	10+81.16	LT-23.04	854.02	845.60	8.42	5' DIAMENTER SAS
SAS #3	13+66.84	LT-3.39	854.52	846.83	7.69	5' DIAMENTER SAS
SAS #4	15+21.85	LT-7.49	854.69	847.68	7.01	5' DIAMENTER SAS
SAS #5	15+77.06	LT-10.69	854.94	848.01	6.93	5' DIAMENTER SAS
SAS #6	17+53.62	LT-7.42	855.88	849.68	6.20	5' DIAMENTER SAS
SAS #8	10+09.57	RT-1.35	854.05	848.00	6.05	4' DIAMENTER SAS

PROPOSED SANITARY PIPES

FROM (DNSTM)	TO (UPSTM)	DWNSTRM E.I.	UPSTRM E.I.	PLAN (PAY) LGTH (FT)	SLOPE (%)	PIPE SIZE	PVC TYPE	* REVISED 7/26/17 MDM NOTES
N. BASSETT STREET								
* SAS#1	SAS #2	845.13	845.24	45	0.24%	21"		ASTM F679, PS115
* SAS #2	SAS #3	845.29	846.83	297	0.52%	18"		ASTM F679, PS115
SAS #3	SAS #4	846.88	847.68	157	0.51%	18"		ASTM F679, PS115
SAS #4	SAS #5	847.73	848.01	56	0.50%	18"		ASTM F679, PS115
SAS #5	SAS #6	848.06	849.68	179	0.91%	15"		AWWA C900 DR 18
* SAS #2	SAS #7	845.48	845.60	23	0.52%	10"		AWWA C900 DR 18
SAS #3	STUB	849.02	849.04	7	0.29%	8"		ASTM D3034 SDR-35
STA 11+25.87 LT 26.31'	STA 11+30.81 LT 26.30'			5		10"		Point Repair(SAG) payable as 10" SDR 35 pipe & comp. couplings
STA 12+64.87 LT 24'	STA 12+74.82 LT 23.99'			10		10"		Point Repair(SAG) payable as 10" SDR 35 pipe & comp. couplings
STA 13+18.82 LT 24.27'	STA 13+23.77 LT 24.26'			5		10"		Point Repair(SAG) payable as 10" SDR 35 pipe & comp. couplings
* SAS#1	SAS#3	845.00	846.83	342	0.54%	18"		ASTM F679, PS115
CONKLIN ALLEY								
EX. 4850-045	4850-044	850.70	851.15	145	0.31%	8" I.D.		OPEN CUT SEWER REPLACE OR PIPE BURST
DAYTON STREET								
* 4951-018	4951-003	845.23	845.60	56	0.66%	10"	D3034 SDR-35	INSTALL 10' LONG 16" DIA. SLEEVE BELOW 6' DIA. STORM STRUCTURE

REMOVE OR ABANDON STRUCTURES

SAS NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	E.I.	DEPTH	NOTES
N. BASSETT STREET						
EX 4951-045	10+24.62	LT-2.39	854.15	845.03	9.12	REM.EX.SAS(6X6 SAS)
EX 4951-005	10+69.12	LT-2.33	854.11	846.06	8.05	REM.EX.SAS(6X6 SAS)
EX 4951-003	10+81.16	LT-23.04	854.10	845.60	8.50	REM.EX.SAS(4' DIA. SAS)
EX 4951-002	13+66.88	RT-0.64	854.49	849.02	5.47	REM.EX.SAS(4' DIA.SAS)
EX 4950-018	15+21.62	LT-6.90	854.72	849.27	5.45	REM.EX.SAS(4' DIA.SAS)
EX 4950-039	15+77.06	LT-10.69	854.85	849.35	5.50	REM.EX.SAS(4' DIA. SAS)
EX 4950-017	17+53.62	LT-7.42	855.88	849.93	5.95	REM.EX.SAS(4' DIA. SAS)

SANITARY SEWER REMOVALS AND ABANDONMENTS

FROM SAS (DWNSTRM)	TO SAS (UPSTREAM)	LENGTH Feet	PAID (Y/N)	PLAN (PAY) LGTH (FT)	SIZE (DIA in)	PIPE TYPE	NOTES
N. BASSETT STREET							
EX 4951-045	EX 4951-005	45	N	0	12"	VP	REMOVE SANITARY
EX 4951-045	EX 4951-003	60	Y	60	10"	RCP	ABANDON EX.10" RCP WITH SLURRY
EX 4951-005	EX 4951-002	297	Y	164	12"	VP	REMOVE SANITARY
EX 4951-002	EX 4950-018	157	Y	63	12"	VP	REMOVE SANITARY
EX 4950-018	EX 4950-039	56	N	0	12"	VP	REMOVE SANITARY
EX 4950-039	EX 4950-017	179	N	0	12"	VP	REMOVE SANITARY
SAS#8	EX 4951-045	15	Y	15	15"	VP	ABANDON EX.15" RCP WITH SLURRY

ADJUST SEWER ACCESS STRUCTURE

SAS NO.	STATION	LOCATION (OFFSET)	EXISTING RIM	PROPOSED RIM	EX. ADJ(ft)	NOTES
EX. 4850-045	40+06.47	RT-0.52	856.15	856.15	0.67	
EX. 4850-044	41+51.35	LT-3.25	858.58	858.58	0.83	REPLACE FRAME & CASTING INCIDENTAL TO ADJUST SAS

UTILITY LINE OPENING

ULO NUMBER	STATION	LOCATION	UTILITY	NOTES
ULO #20	10+62.58	LT-3.32	AT&T	FIBER
ULO #21	13+53.68	LT-3.29	TELEPHONE	FIBER
ULO #22	17+42.76	LT-6.85	MG&E	2 DUCT PACKAGES
ULO #23	13+92.24	LT-2.93	TELEPHONE	FIBER
ULO #24	14+18.23	LT-1.65	TDS	FIBER
ULO #25	17+42.76	LT-6.85	ATC	Contact ATC 3 days prior ULO or arrange for inspection, Doug Vosberg (608)877-7650

5 ADDITIONAL UNDISTRIBUTED ULOs INCLUDED IN THE QUANTITIES

STORM SEWER SCHEDULE

*REV 7/26/2017 KDF
**REV 8/14/2017 KDF

N. BASSETT STREET & ALLEYS 2017		SHEET NO.
PROJECT NO. 11430		U-5
STORM SEWER SCHEDULE		
N. BASSETT ST.	CITY OF MADISON	

PROPOSED STORM STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
BASSETT STREET							
S-1	10+87.50	RT-11.55	TAP	-	850.19	-	-
S-2	11+69.11	RT-11.50	SADDLED H INLET	854.00	850.31	3.69	LP, FP, UD, W/R-3067-7004-VB
S-3	13+14.00	RT-11.50	5X5 SAS	854.33	850.52	3.81	LP, FP, UD, W/R-3067-7004-VB
S-4	11+80.79	LT-11.50	SADDLED H INLET	854.06	847.93	6.13	LP, FP, UD, W/R-3067-7004-VB
S-5	13+14.00	LT-11.50	SADDLED H INLET	854.38	848.13	6.25	LP, FP, UD, W/R-3067-7004-VB
S-6	14+02.18	RT-29.30	H INLET	854.72	851.60	3.12	LP, FP, W/R-3067-7004-VB
** S-7	10+87.00	LT-11.50	SADDLED H INLET	854.01	847.78	6.23	LP, FP, UD, W/R-3067-7004-VB
S-10	14+69.78	RT-11.12	COLLAR	-	851.59	-	-
S-11	14+85.00	RT-12.50	5X5 SAS	855.04	851.61	3.43	LP, FP, UD, W/R-3067-7004-VB
S-11A	14+85.00	LT-11.50	H INLET	854.80	852.21	2.59	LP, FP, UD, W/R-3067-7004-VB
S-12	15+39.49	RT-12.50	5X5 SAS	855.07	851.69	3.38	LP, FP, UD, W/R-3067-7004-VB
* S-12A	15+42.19	LT-19.35	H INLET	854.88	852.63	2.25	LP, FP, UD, W/R-3067-7004-VB
* S-12B	15+57.28	LT-21.08	4X4 SAS	855.00	851.74	3.26	FP, W/R-3067-7004-V
* S-12C	15+77.27	LT-20.41	STORM BEND	-	851.77	-	-
S-13	16+79.95	LT-10.66	5X5 SAS	856.09	851.95	4.14	FP, W/R-3067-7004-V
S-13A	17+10.87	LT-91.04	H INLET	855.84	853.36	2.48	LP, FP, W/R-3067-7004-VB
S-14	17+27.27	LT-11.65	5X5 SAS	856.38	852.03	4.35	FP, W/R-3067-7004-V
S-14A	17+47.50	RT-21.00	TAP	-	853.83	-	-
** S-20	17+81.50	LT-18.49	SADDLED H INLET	855.52	852.11	3.41	LP, FP, W/R-3067-7004-VB
** S-20A	17+77.55	LT-27.67	H INLET	855.62	852.75	2.87	FP, W/R-1550-0054
S-21	17+88.90	LT-19.04	5X5 SAS	855.57	852.12	3.45	FP, W/R-1550-0054
S-22	17+96.61	LT-2.55	STORM BEND	-	852.13	-	-
UNIVERSITY AVENUE							
* S-30	56+05.20	LT-24.00	3X6 SAS	856.82	852.91	3.91	[4]

ADJUST STORM STRUCTURES

STRUC. NO.	ID NO.	STATION	LOCATION (OFFSET)	TYPE	EX TOC	PROP TOC	NOTES
BASSETT STREET							
ADJ-1	AS 4951-017	10+48.50	RT-10.50	SAS	854.12	854.12	NO EXISTING ADJUSTMENT
ADJ-2	AS 4951-021	10+59.25	RT-23.50	SAS	853.71	853.66	NO EXISTING ADJUSTMENT
ADJ-3	AS 4951-020	10+62.65	RT-10.75	SAS	854.09	853.87	[3], NO EXISTING ADJUSTMENT
ADJ-4	AS 4951-022	10+87.50	RT-11.50	SAS	854.21	853.94	[1]
ADJ-5	AS 4951-019	10+48.75	LT-24.50	SAS	853.91	853.91	NO EXISTING ADJUSTMENT
ADJ-6	AS 4951-091	13+44.50	RT-10.75	SAS	854.51	854.28	[3], NO EXISTING ADJUSTMENT
ADJ-7	AS 4951-094	13+91.25	RT-10.75	SAS	854.48	854.48	NO EXISTING ADJUSTMENT

SPECIFIC NOTES

- [1] CONTRACTOR SHALL RECONSTRUCT STRUCTURE ROOF AND ADJUST CASTING TO INLET CASTING. WORK PAID UNDER BID ITEM 90030.
 [2] PIPE SHALL BE TWIN 8" PVC C900. PLAN LENGTH IS TOTAL LENGTH OF BOTH PIPES.
 [3] RECONSTRUCTION OF SAS ROOF WILL BE NEEDED FOR ADJUST. RECONSTRUCTION TO BE PAID UNDER BID ITEM 40371
 * [4] FIELD VERIFY EXISTING STORM PIPE INVERT

PROPOSED STORM PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	DISCH. E.I.	INLET E.I.	PLAN (PAY) LGTH (FT)	PIPE LGTH (FT)	SLOPE (%)	PIPE SIZE	TYPE	NOTES
BASSETT STREET										
P-1	S-1	S-2	850.19	850.31	81.5	77.6	0.15%	24"X38"	HERCP	-
P-2	S-2	S-3	850.31	850.52	145	140.9	0.15%	24"X38"	HERCP	-
P-10	S-10	S-11	851.59	851.61	15.5	12.8	0.16%	24"X38"	HERCP	-
P-11	S-11	S-12	851.61	851.69	48	42.7	0.19%	24"X38"	HERCP	-
P-11A	S-11	S-11A	852.11	852.21	24	20.5	0.50%	12"	TYPE I	-
* P-12	S-12	S-12B	851.69	851.74	38	32.8	0.15%	24"X38"	HERCP	-
* P-12A	S-12	S-12A	852.19	852.63	32	28.4	1.55%	12"	TYPE I	-
* P-12B	S-12B	S-12C	851.74	851.77	20	18	0.17%	24"X38"	HERCP	-
* P-12C	S-12C	S-13	851.77	851.95	107	104.7	0.17%	24"X38"	HERCP	-
P-13	S-13	S-14	851.95	852.03	48	42.9	0.19%	24"X38"	HERCP	-
P-13A	S-13	S-13A	852.95	853.36	86.5	82.2	0.50%	12"	TYPE I	-
P-14A	S-14	S-14A	853.12	853.83	80	35.5	2.00%	8"	PVC C900 [2]	-
* P-20A	S-20	S-20A	852.61	852.75	8.5	7	2.00%	12"	TYPE I	-

NOTE: PLAN LENGTH (PAY LENGTH) IS FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. PIPE LENGTH IS ACTUAL LENGTH OF PIPE FROM STRUCTURE WALL TO STRUCTURE WALL. SLOPE CALCULATED USING PIPE LENGTH.

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES; UD = UNDERDRAIN
 - APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
 - TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS's.
 - TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.
 - ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.
 - SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.
 - ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT KYLE FRANK OF CITY ENGINEERING AT (608) 266-4098 FOR PRECAST APPROVALS, FAX SHOP DRAWINGS TO (608)264-9275, OR EMAIL SHOP DRAWINGS TO KFRANK@CITYOFMADISON.COM.

STORM SEWER SCHEDULE

*REV 7/26/2017 KDF

**REV 8/14/2017 KDF

N. BASSETT STREET & ALLEYS 2017
PROJECT NO. 11430

SHEET NO.
U-6

STORM SEWER SCHEDULE

N. BASSETT ST. CITY OF MADISON

REMOVE STORM STRUCTURES

STRUC. NO.	ID NO.	STATION	LOCATION (OFFSET)	TYPE	NOTES
BASSETT STREET					
R-1	IN 4951-027	10+85.10	RT-19.35	INLET	-
R-2	IN 4951-047	11+86.75	RT-19.35	INLET	[1]
R-3	N/A	12+24.00	RT-10.65	SAS	-
R-4	IN 4951-046	11+87.25	LT-17.90	SADDLED INLET	[1], [2]
R-5	AS 4951-090	13+13.85	LT-14.00	SADDLED SAS	[2]
R-6	IN 4951-043	14+03.70	RT-30.85	INLET	-
** R-7	IN 4951-028	10+81.50	LT-17.75	SADDLED INLET	[2]
R-10	AS 4950-078	14+72.45	RT-11.15	SAS	-
R-11	IN 4950-138	14+73.15	LT-9.15	INLET	-
R-12	IN 4950-034	15+39.70	RT-16.40	INLET	-
R-12A	IN 4950-035	15+42.20	LT-18.90	INLET	-
R-12B	AS 4950-033	15+50.10	LT-8.50	SAS	-
R-12C	AS 4950-033A	15+56.10	LT-11.15	SAS	-
R-21	IN 4950-019	17+88.90	LT-19.05	SAS	-
R-22	IN 4950-027	17+96.60	LT-2.55	INLET	-

REMOVE STORM PIPES

REMOVE NO.	REMOVE FROM	REMOVE TO	LGTH (FT)	PIPE SIZE	PIPE TYPE	PAID (Y/N)	NOTES
BASSETT STREET							
RP-1	ADJ-4	S-3	228	4'X3'	BOX	N	-
RP-10	R-10	R-11	17	12"	VP	Y	-
RP-11	R-10	R-12B	74	24"X38"	HERCP	Y	-
RP-12	R-12C	S-13	106	24"X38"	HERCP	Y	-
RP-12A	R-12A	R-12	9	12"	VP	Y	-
RP-12B	R-12B	R-12	16	12"	VP	Y	-
RP-12C	R-12B	R-12C	3	24"X38"	HERCP	N	-
RP-20	R-21	S-14A	53	15"	VP	Y	-

STORM SEWER ULOs

ULO NO.	STATION	LOCATION (OFFSET)	TYPE	TOP ELEV	NOTES
BASSETT STREET					
ULO1	14+88.95	RT-13.60	GAS	-	-
* ULO2A	15+05.50	RT-18.95	TOP 3 DUCT PKG	851.69	CONFLICT,
* ULO2B	15+05.60	RT-18.85	BTM 3 DUCT PKG	849.88	-
* ULO3A	15+12.75	RT-17.85	3" PLASTIC TELE	850.62	OK, BTM OF STM = 851.33
* ULO3B	15+13.00	RT-17.95	3" PLASTIC TELE	850.63	OK, BTM OF STM = 851.33
* ULO4	15+31.30	RT-6.05	4" PLASTIC TV	850.84	OK, BTM OF STM = 851.36
* ULO5A	15+41.15	LT-9.45	TOP 15 DUCT PKG	851.64	OK, BTM OF STM = 852.13
* ULO5B	15+41.15	LT-9.45	BTM 15 DUCT PKG	849.85	-
* ULO5C	15+40.00	LT-9.65	1" PLASTIC TELE	851.87	OK, BTM OF STM = 852.13
* ULO6	15+42.20	RT-12.75	1" PLASTIC TELE	850.52	OK
* ULO7	15+40.25	RT-2.45	1" PLASTIC TELE	852.19	CONFLICT, STM REVISED
* ULO8A	15+93.55	RT-11.05	6" PLASTIC ELEC	852.86	CONFLICT, STM REVISED
* ULO8B	15+93.85	RT-10.90	6" PLASTIC ELEC	852.40	CONFLICT, STM REVISED
* ULO8C	15+93.75	RT-11.10	6" PLASTIC ELEC	851.86	CONFLICT, STM REVISED
* ULO8D	15+93.95	RT-10.40	TOP 15 DUCT PKG	853.69	CONFLICT, STM REVISED
* ULO8E	15+93.95	RT-10.40	BTM 15 DUCT PKG	850.40	-
* ULO9	17+18.25	LT-9.35	8" STEEL GAS	852.90	[3], CONFLICT
* ULO10	17+34.15	LT-3.85	TOP 15 DUCT PKG	847.74	-

*5 ADDITIONAL UNDISTRIBUTED ULO'S INCLUDED

STORM SEWER ULOs

ULO NO.	STATION	LOCATION (OFFSET)	TYPE	TOP ELEV	NOTES
EX ULO1	17+44.36	RT-12.10	8" WATER	849.89	-
EX ULO2	17+45.33	RT-15.22	10" SAN LAT	853.81	-
EX ULO3	17+45.34	RT-14.50	MGE DUCT PKG	851.37	BOT ELEV = 850.77
EX ULO4	17+50.18	RT-8.71	8" WATER	849.81	-
EX ULO5	17+50.60	RT-9.49	10" SAN LAT	848.70	-
EX ULO6	17+50.68	RT-10.60	MGE DUCT PKG	851.19	BOT ELEV = 847.33
EX ULO7	17+41.27	LT-14.05	MGE DUCT PKG	850.99	BOT ELEV = 849.45

SPECIFIC NOTES

[1] PRIVATE STORM SEWER RECONNECT, TYPE 2 PAID AS SEPARATE BID ITEM 50794

[2] CONTRACTOR SHALL PLUG HOLE IN EX STORM BOX WITH CONCRETE. PLUGGING CONSIDERED INCIDENTAL TO STRUCTURE REMOVAL.

[3] CONTRACTOR SHALL COORDINATE WITH PRIVATE UTILITY OWNER TO RELOCATE AND RESOLVE CONFLICT

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES; UD = UNDERDRAIN

- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.

- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS'S.

- TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.

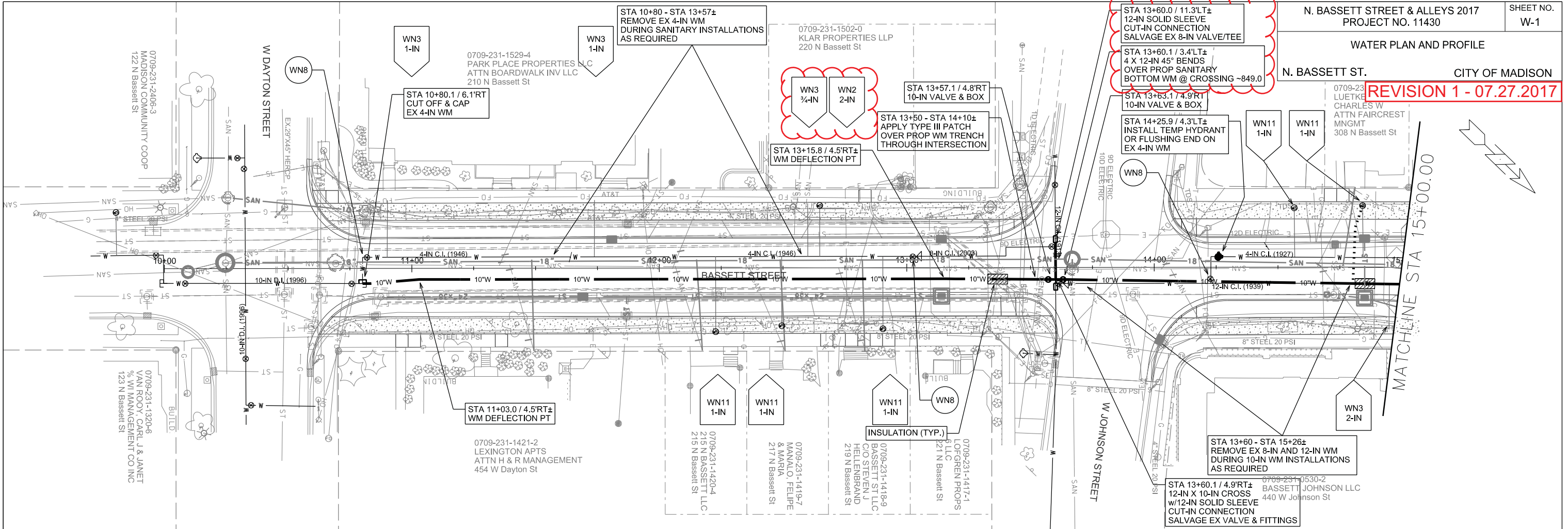
- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.

- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.

- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT KYLE OF CITY ENGINEERING AT (608) 266-4098 FOR PRECAST APPROVALS, FAX SHOP DRAWINGS TO (608)264-9275, OR EMAIL SHOP DRAWINGS TO KFRANK@CITYOFMADISON.COM.

WATER PLAN AND PROFILE
CITY OF MADISON

REVISION 1 - 07.27.2017

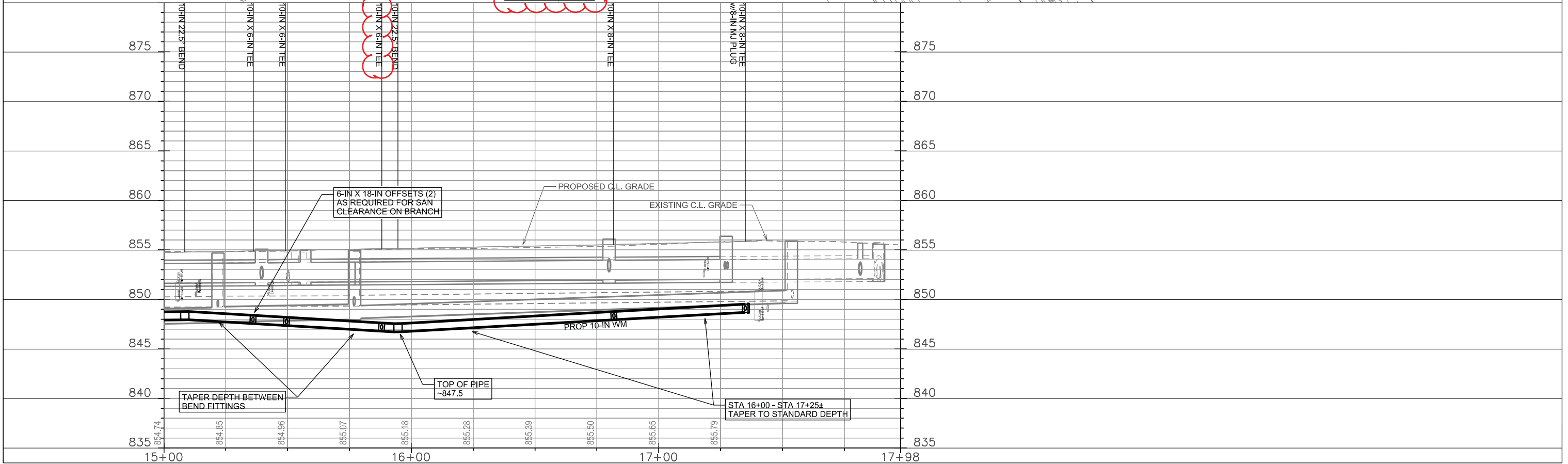
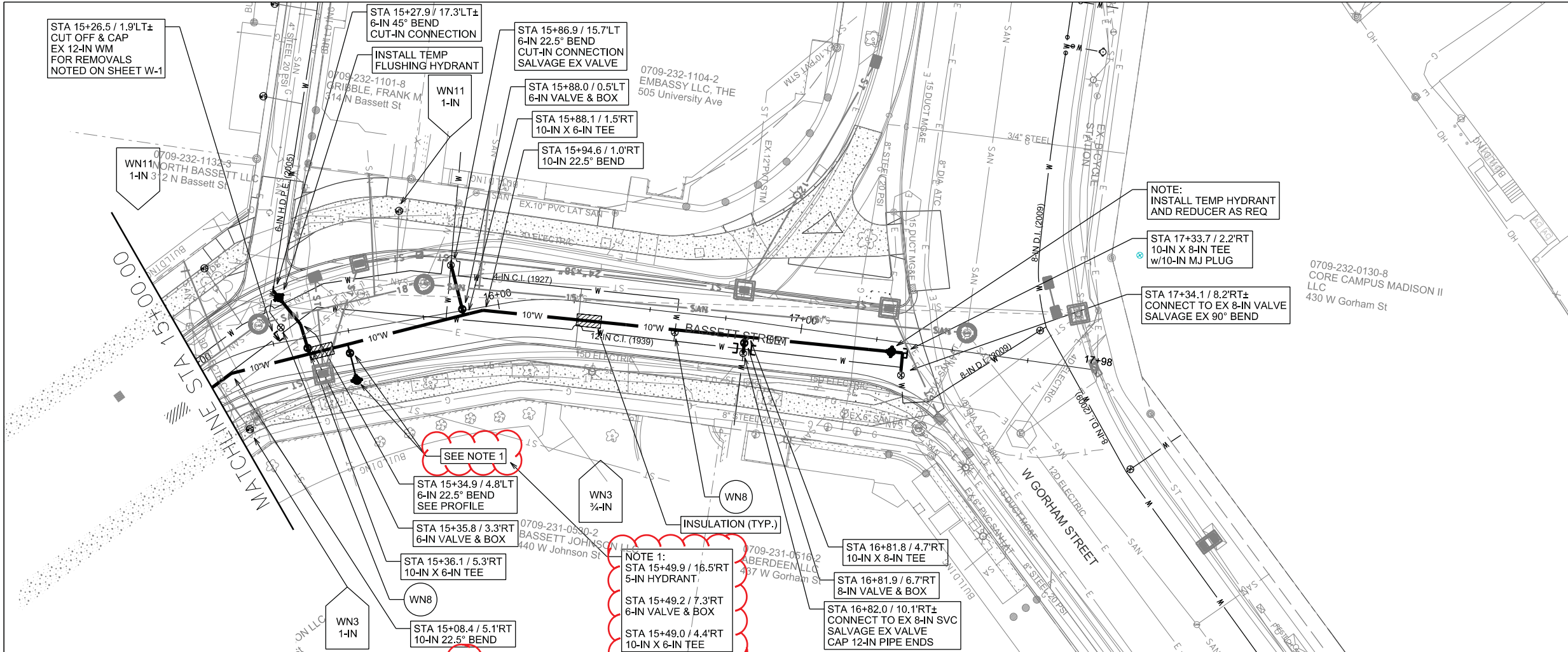


PLOT SCALE:

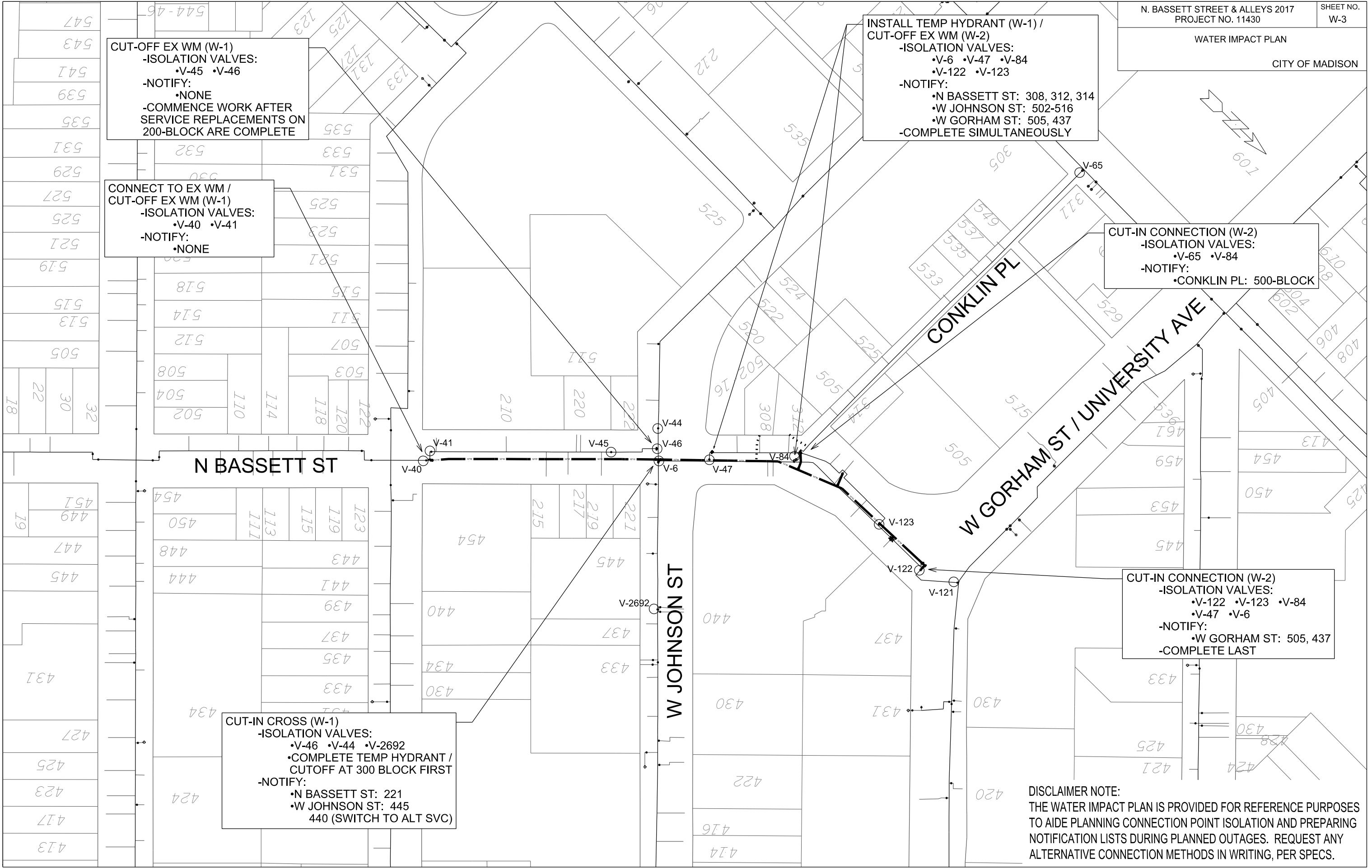
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLOT SCALE: _____ PLOT NAME: _____ REV. DATE: _____ ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CUT-OFF EX WM (W-1)
-ISOLATION VALVES:
•V-45 •V-46
-NOTIFY:
•NONE
-COMMENCE WORK AFTER SERVICE REPLACEMENTS ON 200-BLOCK ARE COMPLETE

CONNECT TO EX WM / CUT-OFF EX WM (W-1)
-ISOLATION VALVES:
•V-40 •V-41
-NOTIFY:
•NONE

INSTALL TEMP HYDRANT (W-1) / CUT-OFF EX WM (W-2)
-ISOLATION VALVES:
•V-6 •V-47 •V-84
•V-122 •V-123
-NOTIFY:
•N BASSETT ST: 308, 312, 314
•W JOHNSON ST: 502-516
•W GORHAM ST: 505, 437
-COMPLETE SIMULTANEOUSLY

CUT-IN CONNECTION (W-2)
-ISOLATION VALVES:
•V-65 •V-84
-NOTIFY:
•CONKLIN PL: 500-BLOCK

CUT-IN CONNECTION (W-2)
-ISOLATION VALVES:
•V-122 •V-123 •V-84
•V-47 •V-6
-NOTIFY:
•W GORHAM ST: 505, 437
-COMPLETE LAST

CUT-IN CROSS (W-1) (1-W)
-ISOLATION VALVES:
•V-2692
•V-44 •V-46
•COMPLETE TEMP HYDRANT / CUTOFF AT 300 BLOCK FIRST
-NOTIFY:
•N BASSETT ST: 221
•W JOHNSON ST: 445
440 (SWITCH TO ALT SVC)

DISCLAIMER NOTE:
THE WATER IMPACT PLAN IS PROVIDED FOR REFERENCE PURPOSES TO AIDE PLANNING CONNECTION POINT ISOLATION AND PREPARING NOTIFICATION LISTS DURING PLANNED OUTAGES. REQUEST ANY ALTERNATIVE CONNECTION METHODS IN WRITING, PER SPECS.

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CONSTRUCTION NOTES:

1. CONSTRUCT NEW WATER MAIN 6.0' BELOW FINISHED GRADE, UNLESS OTHERWISE NOTED. INSULATE MAIN WITH POLYSTYRENE BOARD AT UTILITY CROSSINGS OR OTHER AREAS IDENTIFIED BY ENGINEER AS HAVING INADEQUATE COVER.
2. VERIFY SIZE OF EXISTING WATER SERVICES AND RECONNECT SERVICES AS INDICATED.
3. MINIMIZE DISRUPTION OF SERVICE TO EXISTING CUSTOMERS. NOTIFY PER CONTRACT REQUIREMENTS OF ANY PLANNED WATER OUTAGE.
4. THE EXISTING UTILITIES SHOWN ON THIS PLAN REPRESENT THE BEST INFORMATION AVAILABLE TO THE WATER UTILITY AT THE TIME OF PLAN PREPARATION. CONTRACTOR IS RESPONSIBLE FOR HAVING EACH UTILITY LOCATED PRIOR TO COMMENCING WORK.

- WN1 REPLACE THE EXISTING LEAD SERVICE WITH A NEW COPPER SERVICE.
- WN2 EXTEND AND RECONNECT THE EXISTING COPPER SERVICE TO THE NEW WATER MAIN.
- WN3 EXISTING SERVICE TO BE ABANDONED WHEN THE WATER MAIN IS CUT OFF.
- WN4 DISCONNECT FROM THE OLD WATER MAIN AND RECONNECT THE EXISTING COPPER WATER SERVICE LATERAL TO THE NEW WATER MAIN.
- WN5 RELOCATE THE EXISTING FIRE HYDRANT.
- WN6 ABANDON WATER VALVE ACCESS STRUCTURE.
- WN7 FURNISH AND INSTALL THE NEW TOP SECTION FOR THE WATER ACCESS STRUCTURE.
- WN8 ABANDON THE VALVE BOX.
- WN9 FURNISH THE DITCH, COMPACTION, AND ALL MATERIALS AND LABOR FOR THE INSTALLATION OF NEW SERVICE LATERAL.
- WN10 REMOVE AND SALVAGE EXISTING HYDRANT
- WN11 REPLACE THE EXISTING COPPER SERVICE WITH A COPPER SERVICE

 INDICATES INSULATION AT STORM SEWER CROSSING

ESTIMATE OF PROJECT MATERIALS:

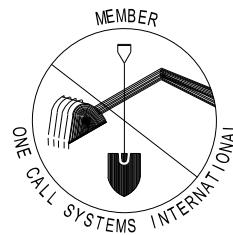
* ESTIMATE OF MATERIALS IS FOR INFORMATION ONLY. ENGINEER DOES NOT GUARANTEE ACCURACY OF MATERIAL TAKE-OFF. ALWAYS REFER TO PLANS.

WATER MAINS	TOTAL
6-IN PIPE (LF)	50
8-IN PIPE (LF)	~15
10-IN PIPE (LF)	660
12-IN PIPE (LF)	<5
POLY WRAP (LF)	820
6-IN VALVES & BOXES	2
8-IN VALVES & BOXES	1
10-IN VALVES & BOXES	3
6-IN - 22.5° BENDS	2
6-IN - 45° BENDS	1
10-IN - 22.5° BENDS	2
10-IN X 6-IN TEES	2
10-IN X 8-IN TEES	2
4-IN MJ PLUGS	1
8-IN MJ PLUGS	1
10-IN MJ PLUGS	1
12-IN MJ CAPS	2
6-IN X 4-IN REDUCERS (TEMP)	1
6-IN X 18-IN OFFSETS	2
12-IN SOLID SLEEVES	1
5-IN HYDRANTS (TEMP)	3
1-IN COPPER TUBING	AS REQ

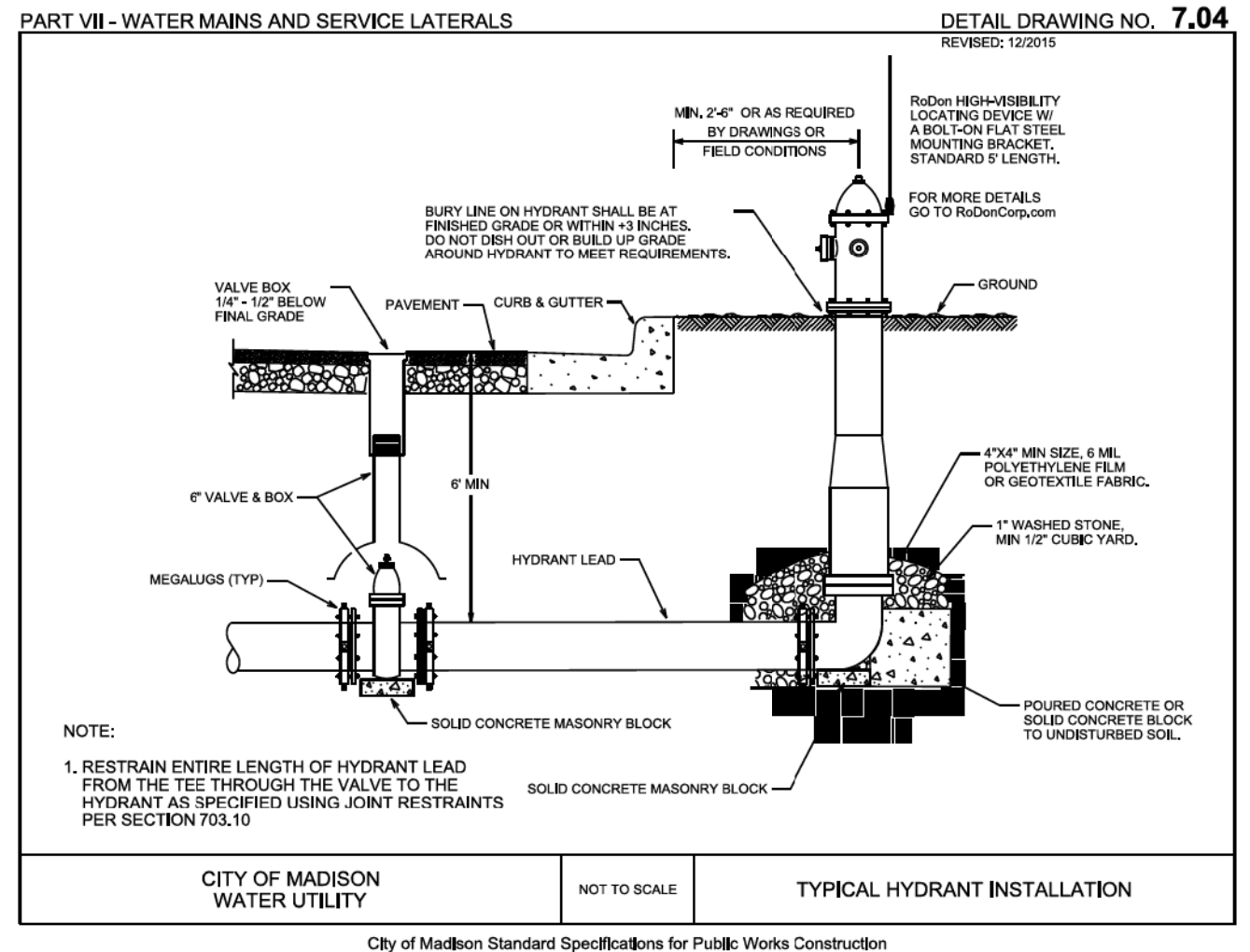
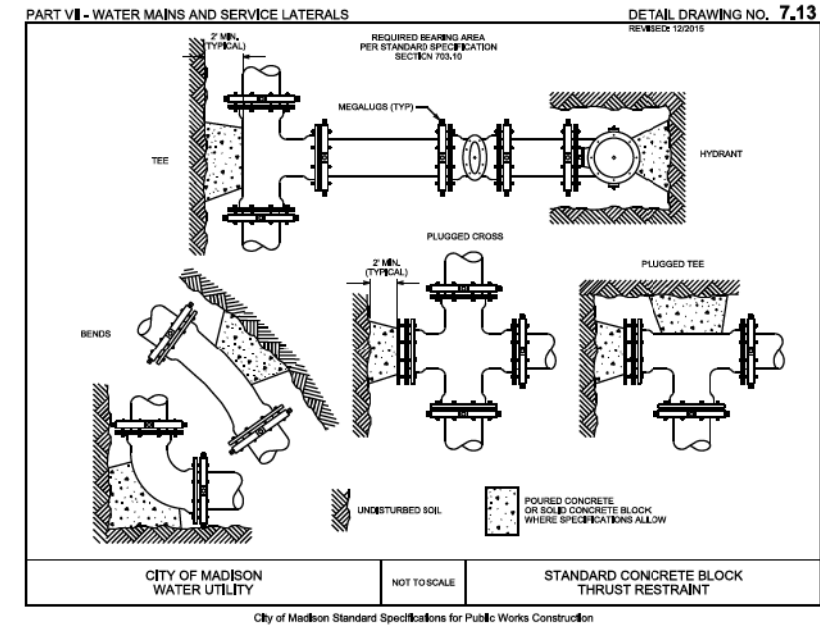
TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE TOLL FREE
811 OR 1-800-242-8511
FAX-A-LOCATE 1-800-338-3860
TDD (FOR HEARING IMPAIRED) 1-800-542-2289

WIS. STATUTE 182.0175 (1974) REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.



DISCLAIMER NOTE:
UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UNDERGROUND AND OVERHEAD UTILITIES PRIOR TO COMMENCING WORK.

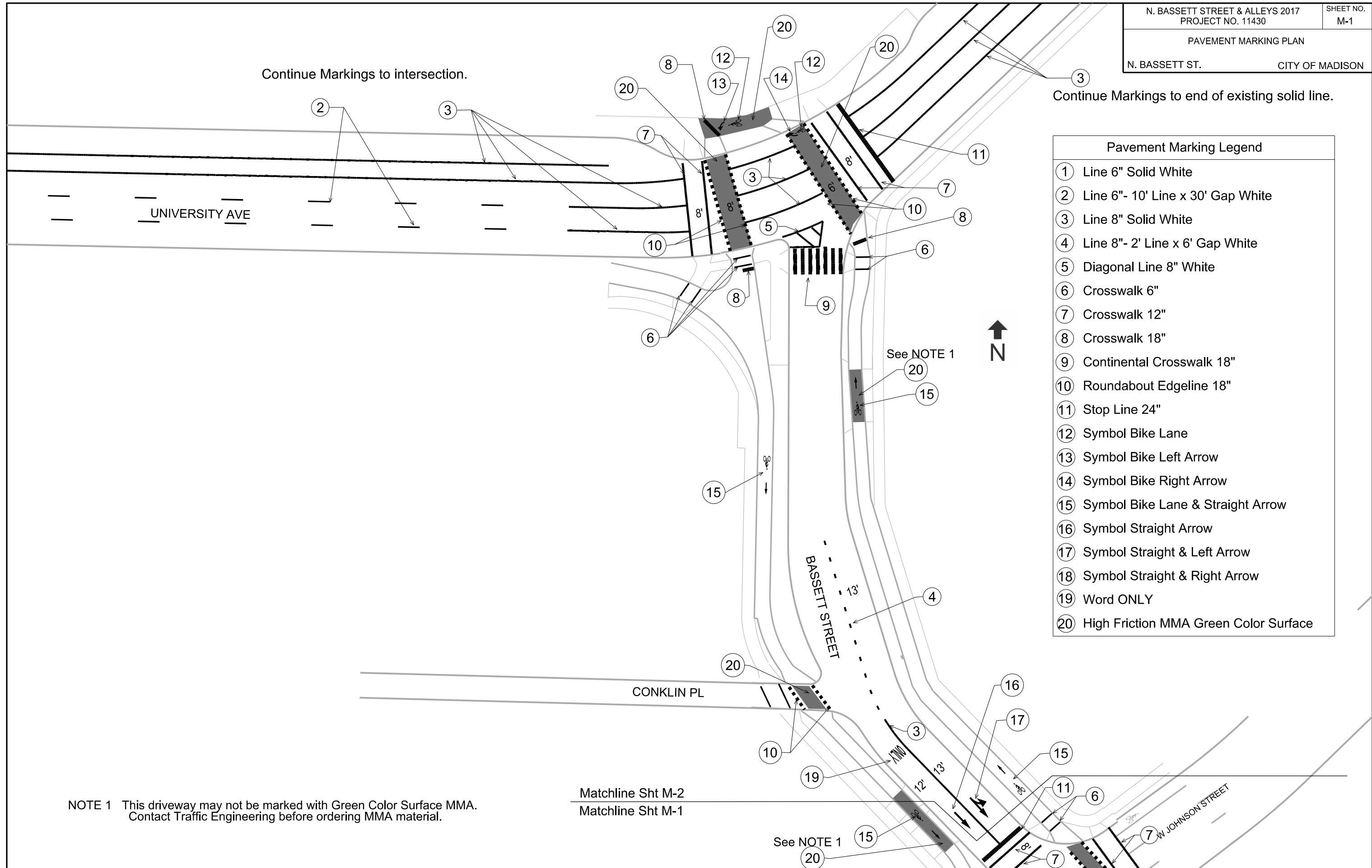


PLOT SCALE: _____ PLOT NAME: _____ REV. DATE: _____ ORIGINATOR: CITY OF MADISON, STREETS DIVISION

Continue Markings to intersection.

Continue Markings to end of existing solid line.

Pavement Marking Legend	
①	Line 6" Solid White
②	Line 6"- 10' Line x 30' Gap White
③	Line 8" Solid White
④	Line 8"- 2' Line x 6' Gap White
⑤	Diagonal Line 8" White
⑥	Crosswalk 6"
⑦	Crosswalk 12"
⑧	Crosswalk 18"
⑨	Continental Crosswalk 18"
⑩	Roundabout Edgeline 18"
⑪	Stop Line 24"
⑫	Symbol Bike Lane
⑬	Symbol Bike Left Arrow
⑭	Symbol Bike Right Arrow
⑮	Symbol Bike Lane & Straight Arrow
⑯	Symbol Straight Arrow
⑰	Symbol Straight & Left Arrow
⑱	Symbol Straight & Right Arrow
⑲	Word ONLY
⑳	High Friction MMA Green Color Surface



NOTE 1 This driveway may not be marked with Green Color Surface MMA.
Contact Traffic Engineering before ordering MMA material.

Matchline Sht M-2
Matchline Sht M-1

See NOTE 1

PLOT SCALE:
PLOT NAME:
REV. DATE:
ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

Matchline Sht M-2
Matchline Sht M-1

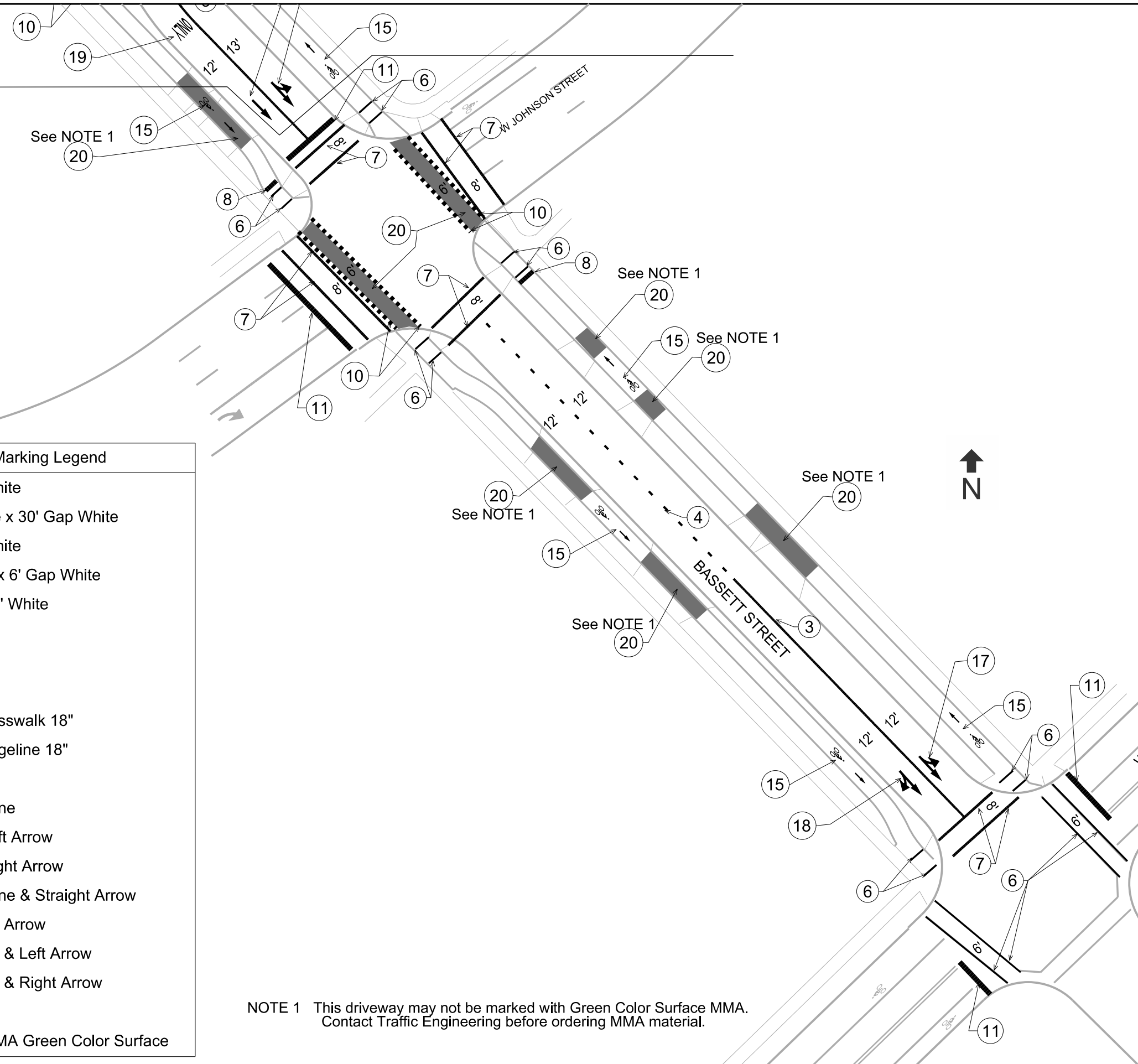
PLOT SCALE:

PLOT NAME:

REV. DATE:
















ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

Pavement Marking Legend	
①	Line 6" Solid White
②	Line 6"- 10' Line x 30' Gap White
③	Line 8" Solid White
④	Line 8"- 2' Line x 6' Gap White
⑤	Diagonal Line 8" White
⑥	Crosswalk 6"
⑦	Crosswalk 12"
⑧	Crosswalk 18"
⑨	Continental Crosswalk 18"
⑩	Roundabout Edgeline 18"
⑪	Stop Line 24"
⑫	Symbol Bike Lane
⑬	Symbol Bike Left Arrow
⑭	Symbol Bike Right Arrow
⑮	Symbol Bike Lane & Straight Arrow
⑯	Symbol Straight Arrow
⑰	Symbol Straight & Left Arrow
⑱	Symbol Straight & Right Arrow
⑲	Word ONLY
⑳	High Friction MMA Green Color Surface



NOTE 1 This driveway may not be marked with Green Color Surface MMA.
Contact Traffic Engineering before ordering MMA material.

LEGEND

-  2-3" CONDUIT (UNLESS NOTED AS OTHER)
-  1-2" CONDUIT (UNLESS NOTED AS OTHER)
-  LB-1 BASE
-  LB-3 BASE (LB-8 WHEN NOTED)
-  TYPE G BASE
-  HANDHOLE TYPE 1
-  HANDHOLE TYPE 3
-  HANDHOLE TYPE 5
-  HANDHOLE TYPE 7
-  TYPE P CABINET BASE
-  TYPE 13 BASE
-  EXISTING TYPE G BASE
-  EXISTING HANHOLE
-  EXISTING CABINET
-  WOOD POLE

GENERAL ELECTRIC NOTES

1. ALL LOCATIONS ARE APPROXIMATE. THE TRAFFIC ENGINEER SHALL APPROVE FINAL LOCATIONS, INCLUDING SETBACK, IN THE FIELD. THE CONTRACTOR SHALL NOTIFY GRETCHEN AVILES (266-4899) CITY TRAFFIC ENGINEERING, AT LEAST 24-HOURS IN ADVANCE OF NEEDING CONDUIT OR BASE LOCATIONS MARKED.
2. BASES INSTALLED IN TERRACE SHALL BE 4' FROM FACE OF CURB UNLESS OTHERWISE NOTED. SUBJECT TO NOTE 1 ABOVE.
3. THE CONTRACTOR SHALL DO ALL WORK IN ACCORDANCE WITH "CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 2017 EDITION" AND ALL ADDENDUMS THERETO. ALL CONDUITS SHALL BE PVC, SCHEDULE 80 UNDER PAVEMENT OR SCHEDULE 40 OTHERWISE. PULL WIRE REQUIRED AS PER STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL CALL TROY VANT (395-1975) AT THE TRAFFIC ENGINEERING SHOP AT LEAST 24-HOURS IN ADVANCE OF POURING BASES OR BURYING CONDUIT TO ARRANGE FOR INSPECTION. ANY WORK COMPLETED WITHOUT INSPECTION IS SUBJECT TO REJECTION.
5. THE CONTRACTOR SHALL ARRANGE FOR PICKUP OF THE FOLLOWING CITY FURNISHED MATERIALS, WHICH SHOULD BE ARRANGED FOR PICKUP BY CALLING DENNIS ROWE, TRAFFIC ENGINEERING SHOP, 266-9034, 1120 SAYLE ST, AT LEAST 24-HOURS PRIOR TO NEEDING MATERIALS:

3/4"X19"	ANCHOR BOLTS: 7 SETS OF 4	FOR TYPE G BASES
3/4"X24"	ANCHOR BOLTS: 9 SETS OF 4	FOR LB-1 BASES
1"X40"	ANCHOR BOLTS: 5 SETS OF 4	FOR LB-3 BASES
1-1/4"X48"	ANCHOR BOLTS: 1 SET OF 4	FOR LB-8 BASES
6. THE CONTRACTOR SHALL INSTALL LOOP LEED DUCT CONDUITS PRIOR TO CITY CREWS INSTALLING LOOP DETECTOR WIRES.
7. NOTIFY TOM BODENSTEIN (266-4767) A MINIMUM OF 24 HOURS AND MAXIMUM OF 48 HOURS PRIOR TO FINAL BASE COURSE COMPACTION AND TRIMMING FOR CITY CREWS TO INSTALL LOOP DETECTION WIRES IN THE BASE COURSE PRIOR TO PLACEMENT OF ASPHALTIC PAVEMENT.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

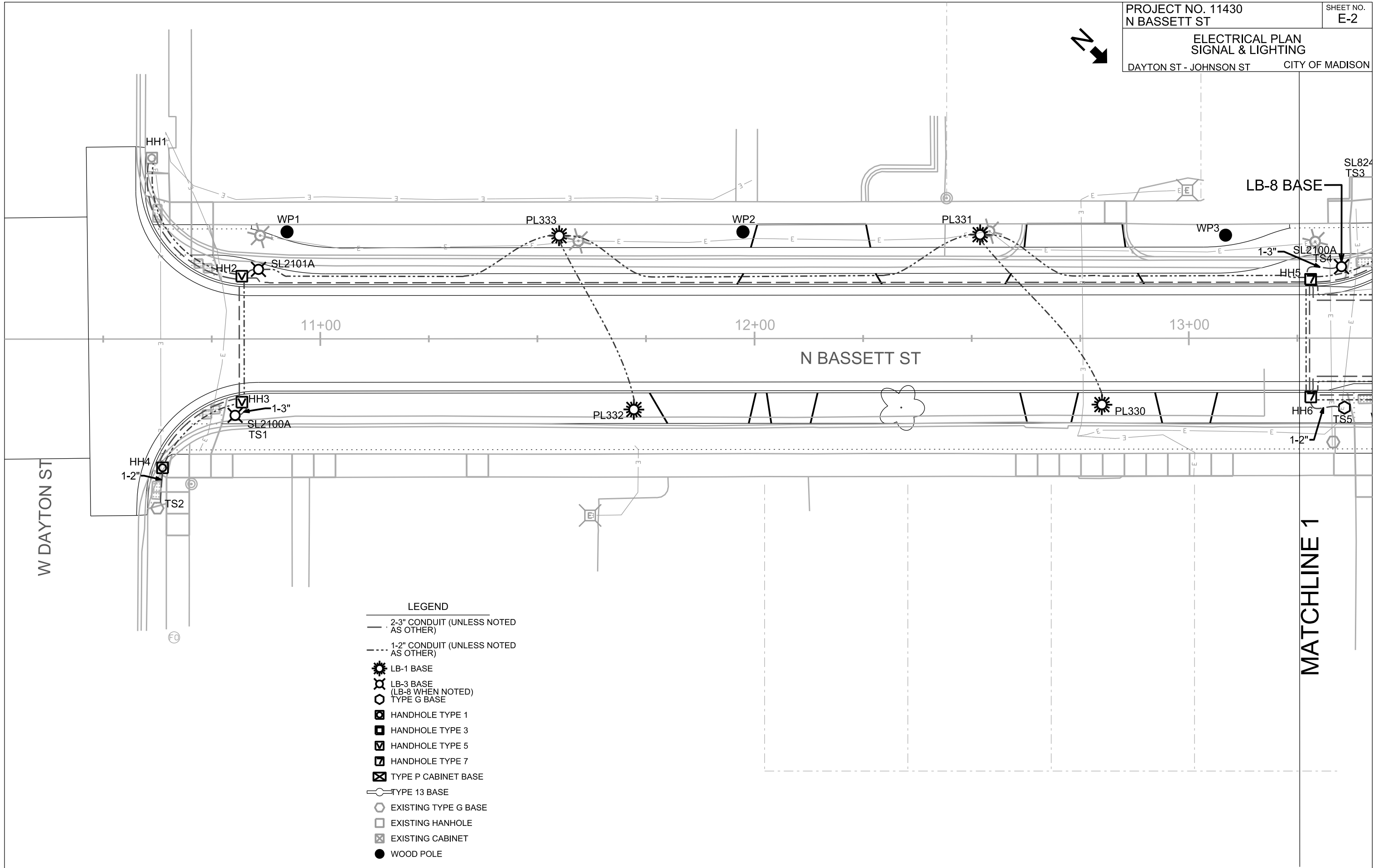


PLOT SCALE:

PLOT NAME:

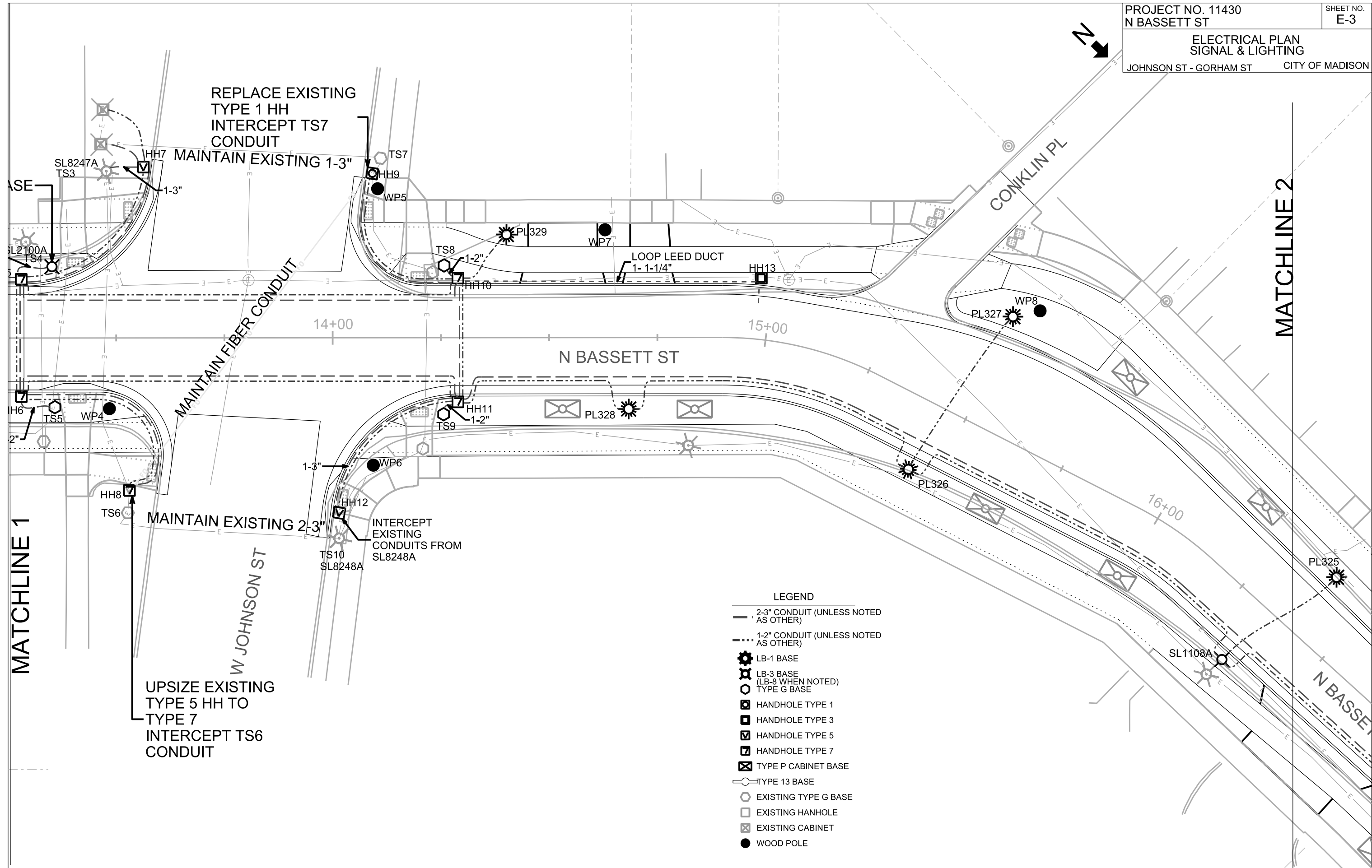
REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



LEGEND

- 2-3" CONDUIT (UNLESS NOTED AS OTHER)
- - - 1-2" CONDUIT (UNLESS NOTED AS OTHER)
- ⊙ LB-1 BASE
- ⊗ LB-3 BASE (LB-8 WHEN NOTED)
- ⊙ TYPE G BASE
- ⊠ HANDHOLE TYPE 1
- ⊠ HANDHOLE TYPE 3
- ⊠ HANDHOLE TYPE 5
- ⊠ HANDHOLE TYPE 7
- ⊠ TYPE P CABINET BASE
- ⊖ TYPE 13 BASE
- ⊙ EXISTING TYPE G BASE
- ⊠ EXISTING HANHOLE
- ⊠ EXISTING CABINET
- WOOD POLE



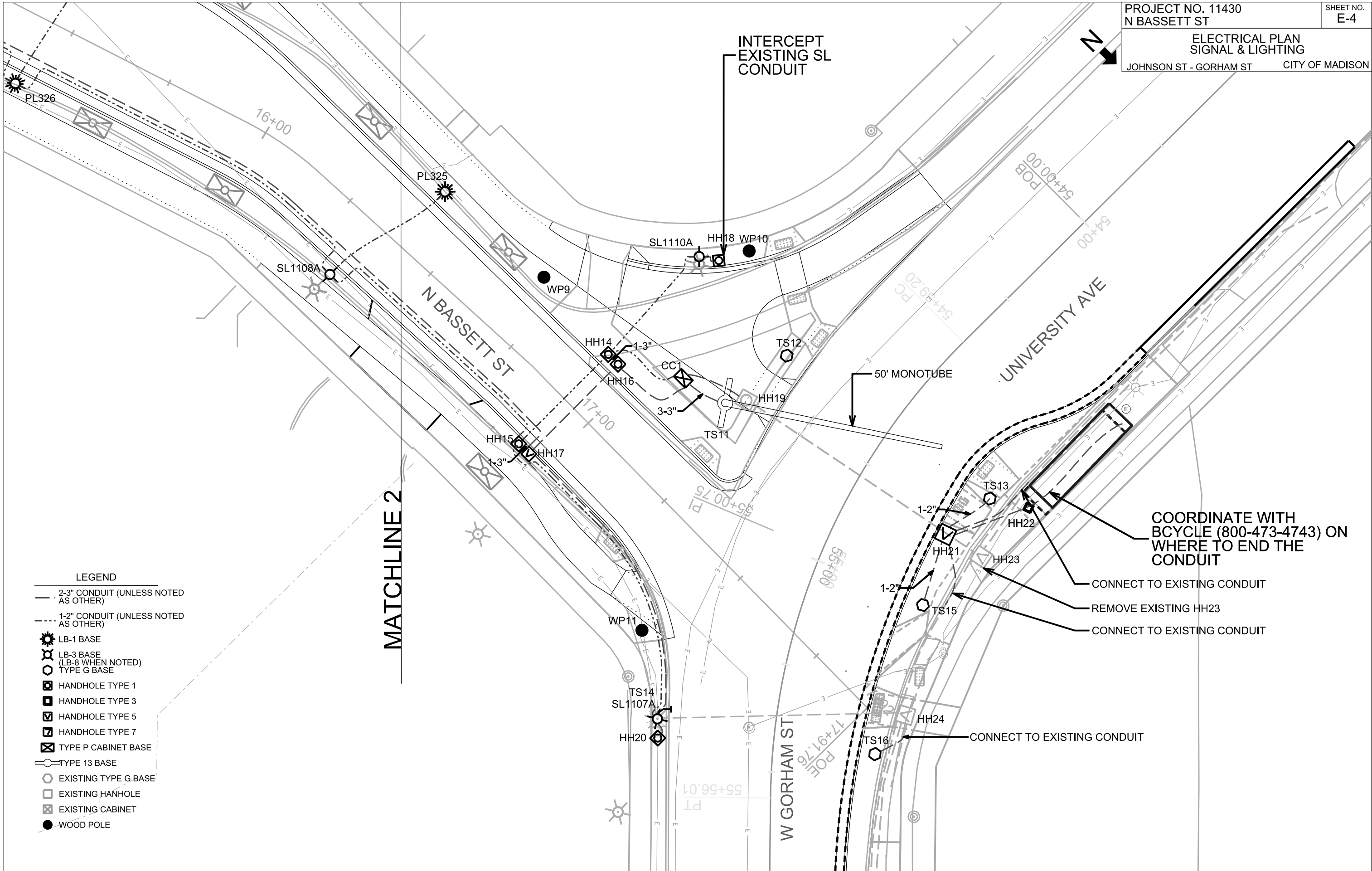
- LEGEND**
- 2-3" CONDUIT (UNLESS NOTED AS OTHER)
 - - - 1-2" CONDUIT (UNLESS NOTED AS OTHER)
 - ⚙️ LB-1 BASE
 - ⚙️ LB-3 BASE (LB-8 WHEN NOTED) TYPE G BASE
 - ⊠ HANDHOLE TYPE 1
 - ⊠ HANDHOLE TYPE 3
 - ⊠ HANDHOLE TYPE 5
 - ⊠ HANDHOLE TYPE 7
 - ⊠ TYPE P CABINET BASE
 - TYPE 13 BASE
 - EXISTING TYPE G BASE
 - EXISTING HANHOLE
 - ⊠ EXISTING CABINET
 - WOOD POLE

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



MATCHLINE 2

LEGEND

- 2-3" CONDUIT (UNLESS NOTED AS OTHER)
- - - 1-2" CONDUIT (UNLESS NOTED AS OTHER)
- ⚙️ LB-1 BASE
- ⚙️ LB-3 BASE (LB-8 WHEN NOTED)
- ⊙ TYPE G BASE
- ⊠ HANDHOLE TYPE 1
- ⊠ HANDHOLE TYPE 3
- ⊠ HANDHOLE TYPE 5
- ⊠ HANDHOLE TYPE 7
- ⊠ TYPE P CABINET BASE
- ⊙ TYPE 13 BASE
- ⊙ EXISTING TYPE G BASE
- ⊠ EXISTING HANDHOLE
- ⊠ EXISTING CABINET
- WOOD POLE

PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

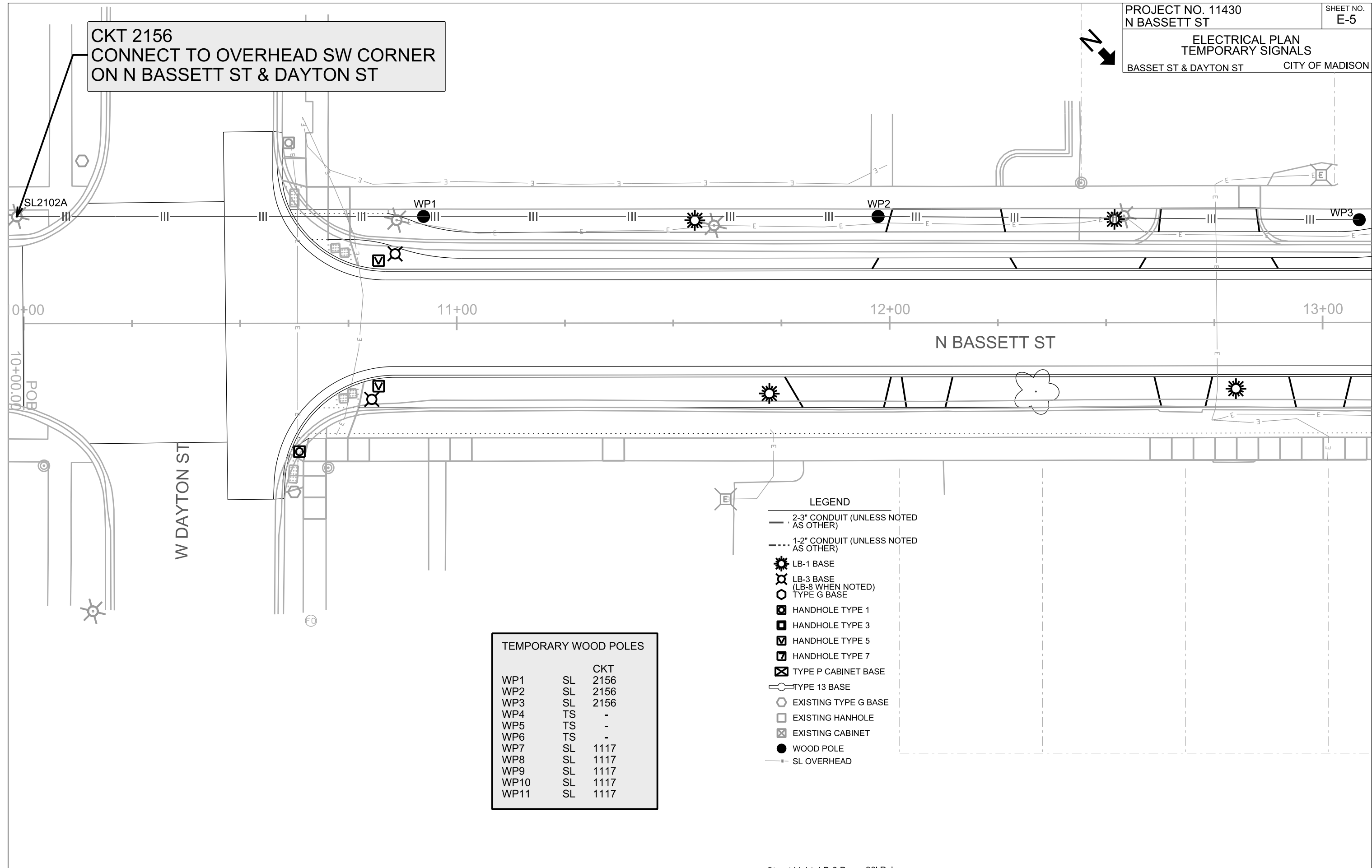
CKT 2156
CONNECT TO OVERHEAD SW CORNER
ON N BASSETT ST & DAYTON ST

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



TEMPORARY WOOD POLES		
		CKT
WP1	SL	2156
WP2	SL	2156
WP3	SL	2156
WP4	TS	-
WP5	TS	-
WP6	TS	-
WP7	SL	1117
WP8	SL	1117
WP9	SL	1117
WP10	SL	1117
WP11	SL	1117

- LEGEND**
- 2-3" CONDUIT (UNLESS NOTED AS OTHER)
 - - - 1-2" CONDUIT (UNLESS NOTED AS OTHER)
 - ☀ LB-1 BASE
 - ⊗ LB-3 BASE (LB-8 WHEN NOTED)
 - ⬡ TYPE G BASE
 - ⊠ HANDHOLE TYPE 1
 - ⊠ HANDHOLE TYPE 3
 - ⊠ HANDHOLE TYPE 5
 - ⊠ HANDHOLE TYPE 7
 - ⊠ TYPE P CABINET BASE
 - ⊙ TYPE 13 BASE
 - ⬡ EXISTING TYPE G BASE
 - ⊠ EXISTING HANHOLE
 - ⊠ EXISTING CABINET
 - WOOD POLE
 - SL OVERHEAD

CKT 8247
 OVERHEAD FROM SL8247A TO WP5,
 CONNECT WEST TO SL8246M
 FROM WP5 TO SL8248A, CONNECT
 EAST TO SL1742M

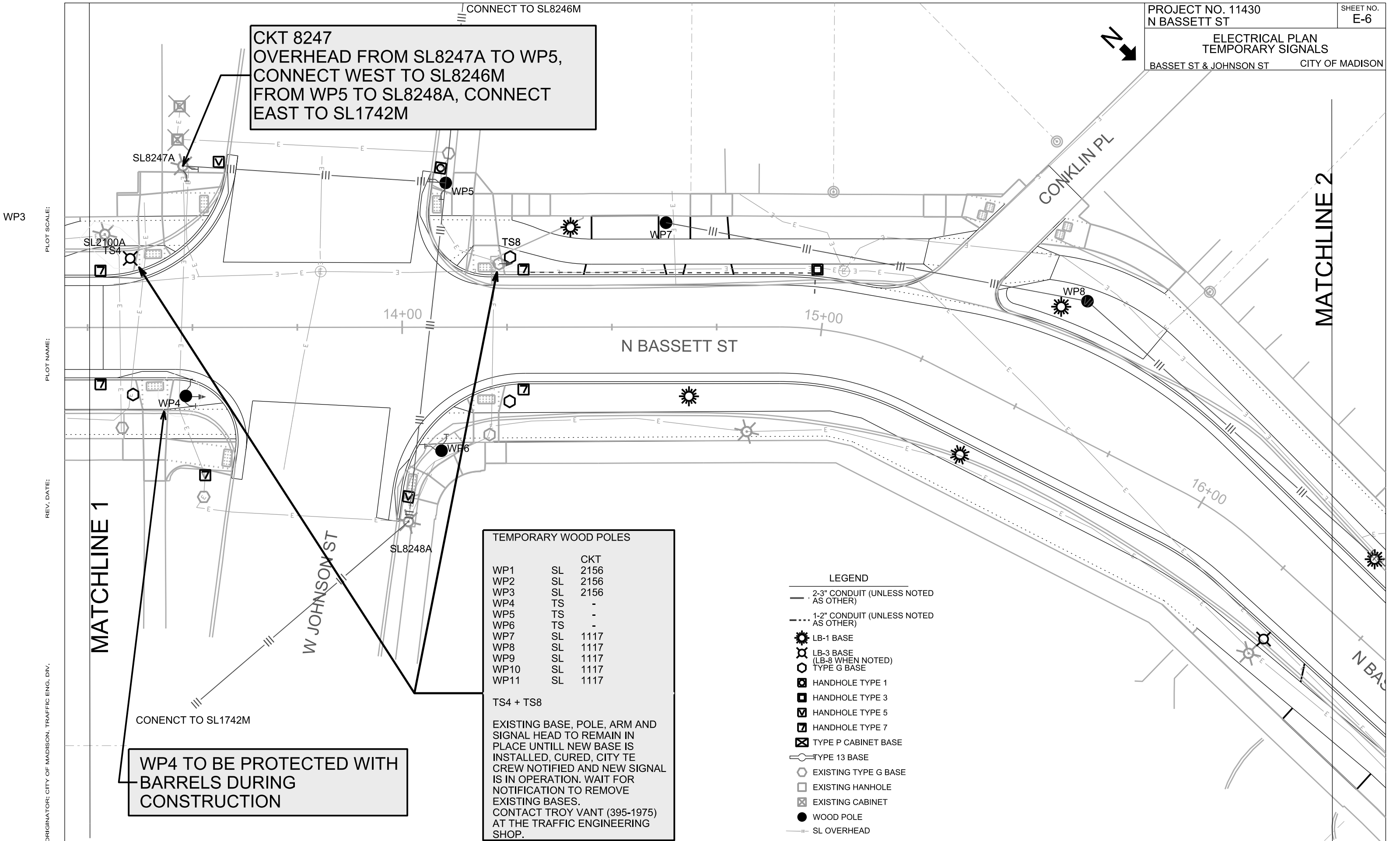
WP4 TO BE PROTECTED WITH
 BARRELS DURING
 CONSTRUCTION

TEMPORARY WOOD POLES		
WP1	SL	2156
WP2	SL	2156
WP3	SL	2156
WP4	TS	-
WP5	TS	-
WP6	TS	-
WP7	SL	1117
WP8	SL	1117
WP9	SL	1117
WP10	SL	1117
WP11	SL	1117

TS4 + TS8

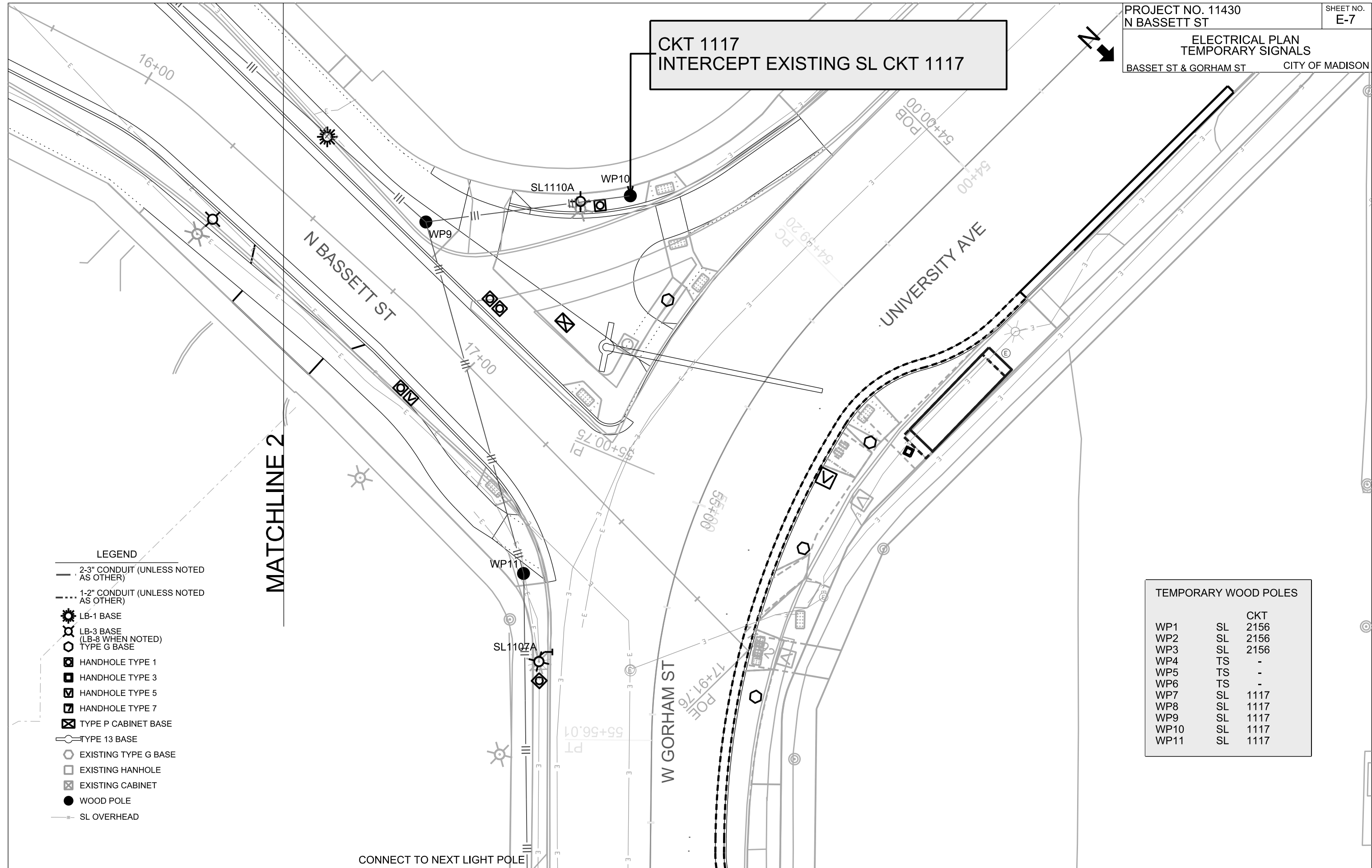
EXISTING BASE, POLE, ARM AND SIGNAL HEAD TO REMAIN IN PLACE UNTILL NEW BASE IS INSTALLED, CURED, CITY TE CREW NOTIFIED AND NEW SIGNAL IS IN OPERATION. WAIT FOR NOTIFICATION TO REMOVE EXISTING BASES. CONTACT TROY VANT (395-1975) AT THE TRAFFIC ENGINEERING SHOP.

LEGEND	
	2-3" CONDUIT (UNLESS NOTED AS OTHER)
	1-2" CONDUIT (UNLESS NOTED AS OTHER)
	LB-1 BASE
	LB-3 BASE (LB-8 WHEN NOTED)
	TYPE G BASE
	HANDHOLE TYPE 1
	HANDHOLE TYPE 3
	HANDHOLE TYPE 5
	HANDHOLE TYPE 7
	TYPE P CABINET BASE
	TYPE 13 BASE
	EXISTING TYPE G BASE
	EXISTING HANHOLE
	EXISTING CABINET
	WOOD POLE
	SL OVERHEAD



PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

CKT 1117
 INTERCEPT EXISTING SL CKT 1117



LEGEND

- 2-3" CONDUIT (UNLESS NOTED AS OTHER)
- - - 1-2" CONDUIT (UNLESS NOTED AS OTHER)
- ⊗ LB-1 BASE
- ⊗ LB-3 BASE (LB-8 WHEN NOTED) TYPE G BASE
- ⊠ HANDHOLE TYPE 1
- ⊠ HANDHOLE TYPE 3
- ⊠ HANDHOLE TYPE 5
- ⊠ HANDHOLE TYPE 7
- ⊠ TYPE P CABINET BASE
- ⊗ TYPE 13 BASE
- ⊗ EXISTING TYPE G BASE
- ⊠ EXISTING HANHOLE
- ⊠ EXISTING CABINET
- WOOD POLE
- SL OVERHEAD

TEMPORARY WOOD POLES

WP	SL	CKT
WP1	SL	2156
WP2	SL	2156
WP3	SL	2156
WP4	TS	-
WP5	TS	-
WP6	TS	-
WP7	SL	1117
WP8	SL	1117
WP9	SL	1117
WP10	SL	1117
WP11	SL	1117

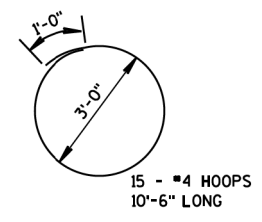
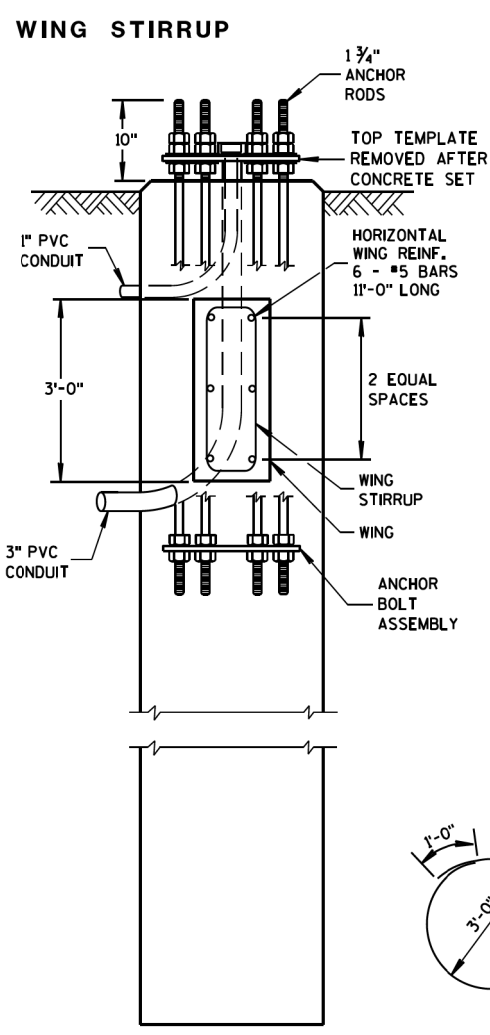
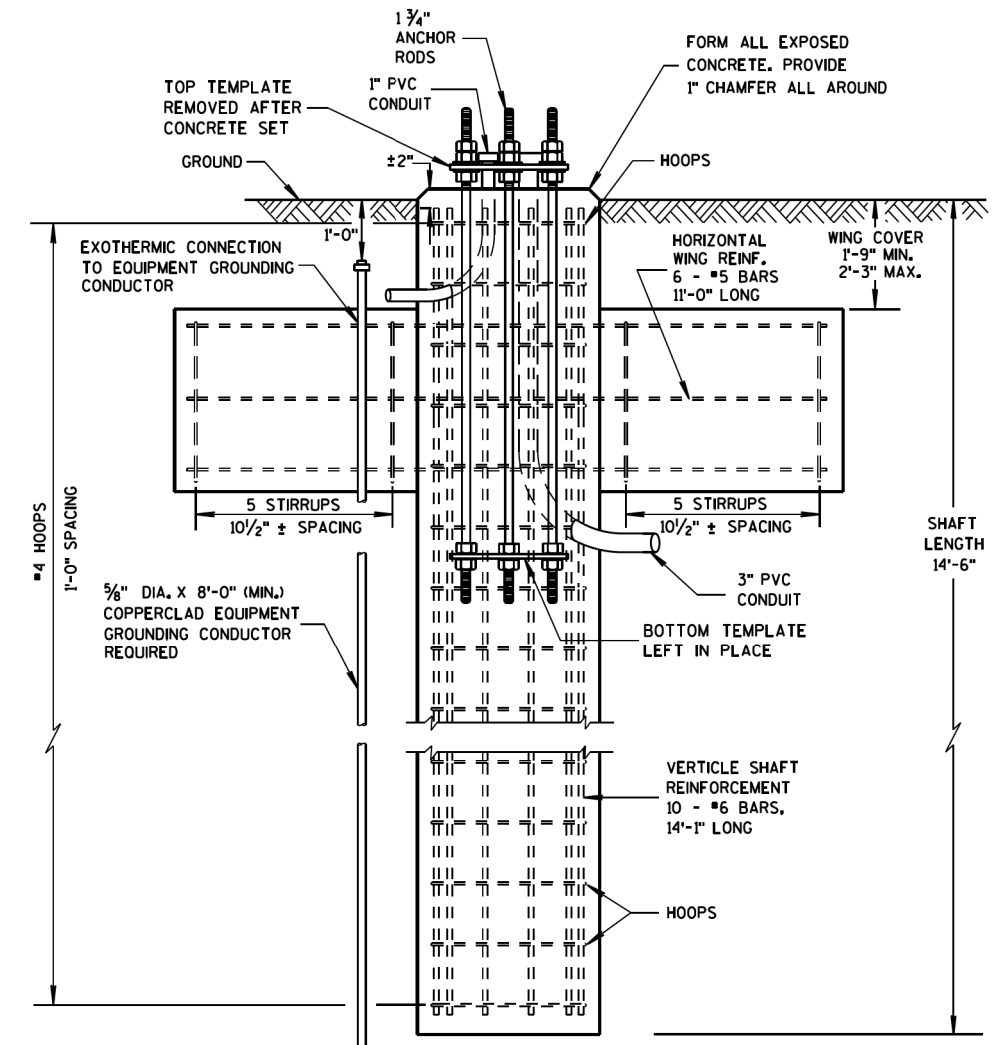
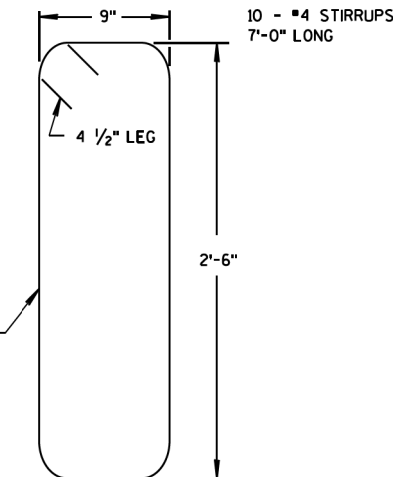
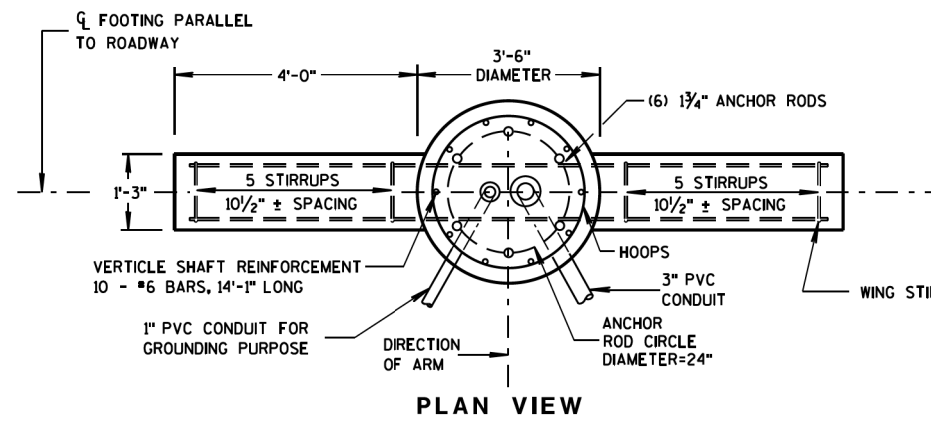
PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

CONNECT TO NEXT LIGHT POLE



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ORIENT ANCHOR RODS IN FOOTING AND PROVIDE ANCHOR ROD PROJECTION ABOVE TOP OF CONCRETE FOOTING BASE PER THIS SHEET.

BENDING DIMENSIONS FOR REINFORCING BARS ARE OUT TO OUT.

USE 3" CLEAR FOR ALL REINFORCEMENT UNLESS NOTED OTHERWISE.

THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF THE UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.

WELDING OF ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TEMPLATES SHALL BE USED.

BASES (SHAFT), BELOW THE WING, SHALL BE EXCAVATED BY THE USE OF A CIRCULAR AUGER, IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE SOIL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

TOP SURFACE OF THE CONCRETE BASE SHALL BE TROWEL FINISHED AND LEVEL.

CONDUIT SIZE AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6 X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASE SHALL BE 4 1/2" INCHES. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED. NONMETALLIC CONDUIT SHALL HAVE BELL ENDS INSTALLED. ALL CONDUIT SHALL SLOPE TO PULL BOX.

ALL CONDUIT ENDS AT THE TOP OF THE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION OF CABLE OR WIRE.

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTOR FITTINGS, UL LISTED FOR ELECTRICAL USE, SHALL BE USED.

A NO. 4 AWG, STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD).

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER THE BASE THROUGH A 1-INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4-FOOT COIL OF WIRE ABOVE THE CONCRETE BASE, THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS.

THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVEL WAY SHALL BE 24-INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18-INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36-INCHES, (GREATER THAN 36-INCHES IF INSTALLED IN BREAKER-RUN), EXCEPT WITH THE WRITTEN APPROVAL OF THE ENGINEER.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1/40 FROM VERTICAL.

CONCRETE MASONRY	fc=3,500 p.s.i.
HIGH STRENGTH BAR STEEL REINFORCEMENT, GRADE 60	fy=60,000 p.s.i.
ANCHOR RODS, ASTM F1554 GRADE 55 (IN ACCORDANCE WITH SECTION 641.2.2.3 OF THE STANDARD SPECIFICATIONS)	fy=55,000 p.s.i.
TEMPLATES, ASTM A709 GRADE 36	fy=36,000 p.s.i.

(FOR TYPE 12 & 13 POLES)

CONCRETE = 6.3 C.Y.
H.S. REINFORCEMENT = 433 LBS.

TO BE USED WHEN GROUND ELEVATION AT BASE EQUALS OR IS GREATER THAN HIGH POINT OF ROADWAY ELEVATION.
SEE S.D.D. 9C13-2 WHEN GROUND ELEVATION AT BASE IS LOWER THAN HIGH POINT OF ROADWAY ELEVATION.

CONCRETE BASE TYPE 13
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

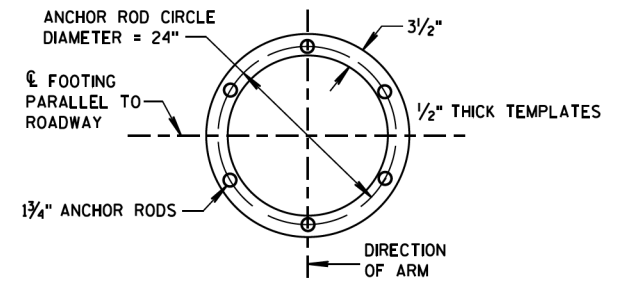
PLOT SCALE: PLOT NAME: REV. DATE: ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

6

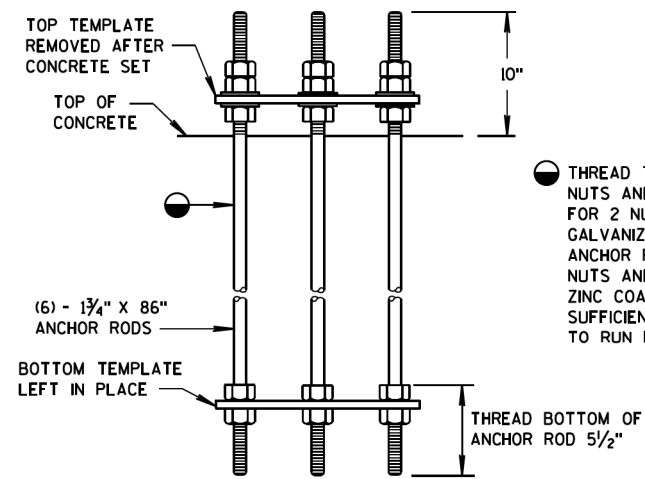
6

S.D.D. 9 C 12-8a

S.D.D. 9 C 12-8a

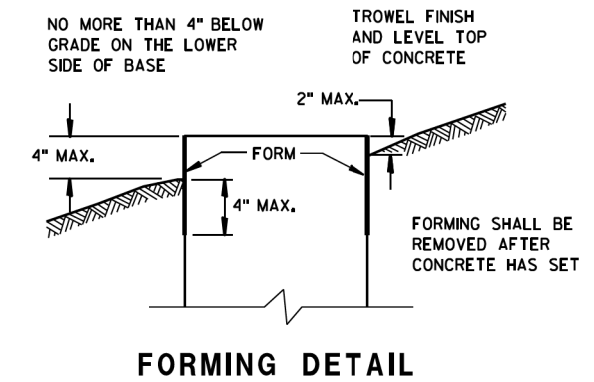


TOP AND BOTTOM TEMPLATES



ANCHOR BOLT ASSEMBLY DETAIL

CONCRETE BASE TYPE 13 ANCHOR ASSEMBLY



FORMING DETAIL

6

6

S.D.D. 9 C 12-8b

S.D.D. 9 C 12-8b

CONCRETE BASE TYPE 13

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2016 /S/ Ahmet Demirbilek
DATE STATE ELECTRICAL ENGINEER
FHWA

TRAFFIC CONTROL PLAN

UNIVERSITY AVE. CITY OF MADISON

Rev. 5/17/17 TAM



PLOT SCALE:
PLOT NAME:
REV. DATE:
ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.





Two Left Lanes Closed

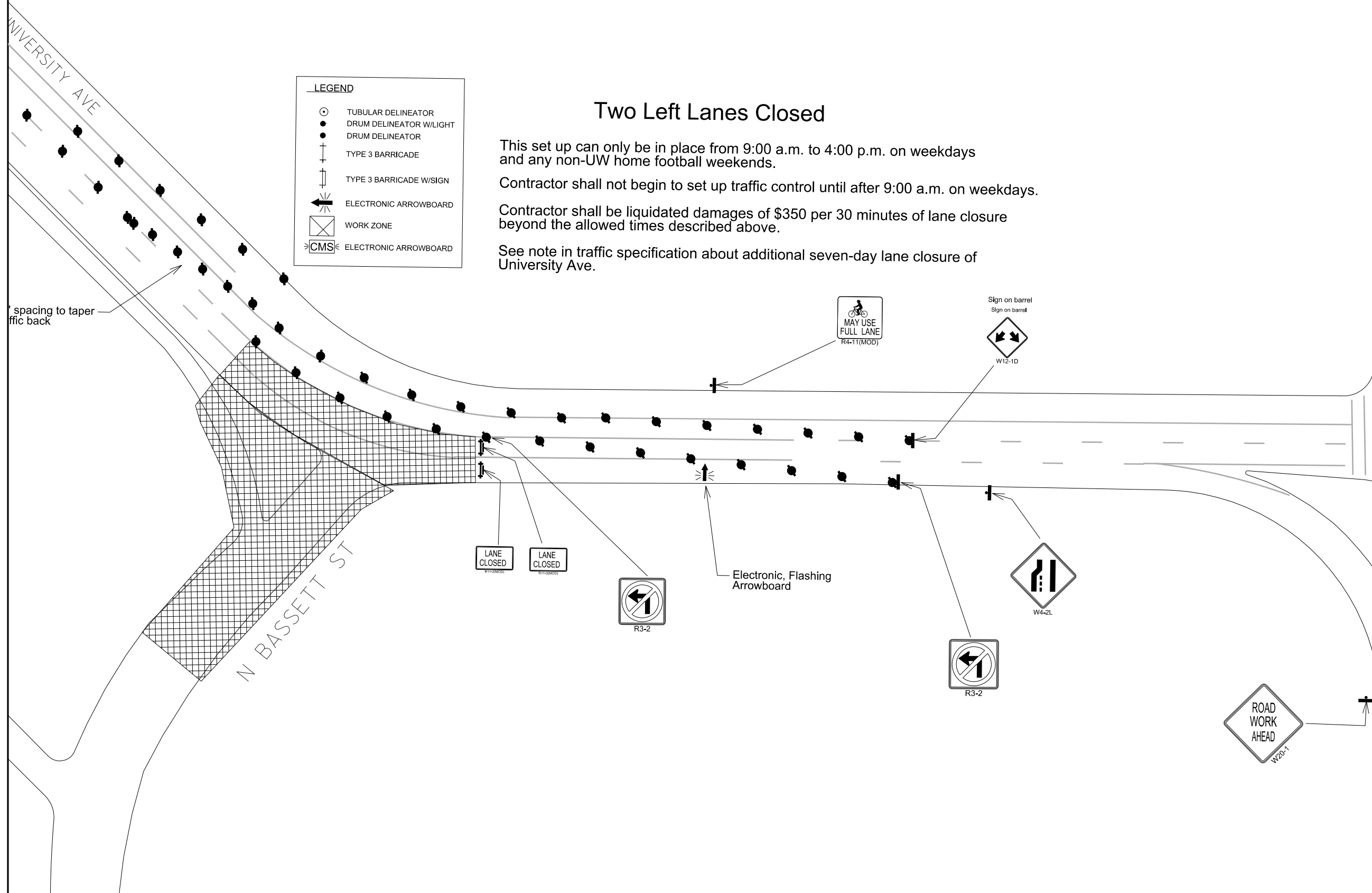
This set up can only be in place from 9:00 a.m. to 4:00 p.m. on weekdays and any non-UW home football weekends.

Contractor shall not begin to set up traffic control until after 9:00 a.m. on weekdays.

Contractor shall be liquidated damages of \$350 per 30 minutes of lane closure beyond the allowed times described above.

See note in traffic specification about additional seven-day lane closure of University Ave.

LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	ELECTRONIC ARROWBOARD



PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

Rev. 5/17/17 TAM



Middle Two Lanes Closed

This set up can only be in place from 9:00 a.m. to 4:00 p.m. on weekdays and any non-UW home football weekends.

Lanes must be paved or plated to open up to three lanes by 4:00 p.m. on each weekday. Lane closures may not begin to be set up until 9:00 a.m. on weekdays.

Contractor shall be liquidated damages of \$350 per 30 minutes of lane closure beyond the allowed times described above.

See note in traffic specification about additional seven-day lane closure of University Ave.

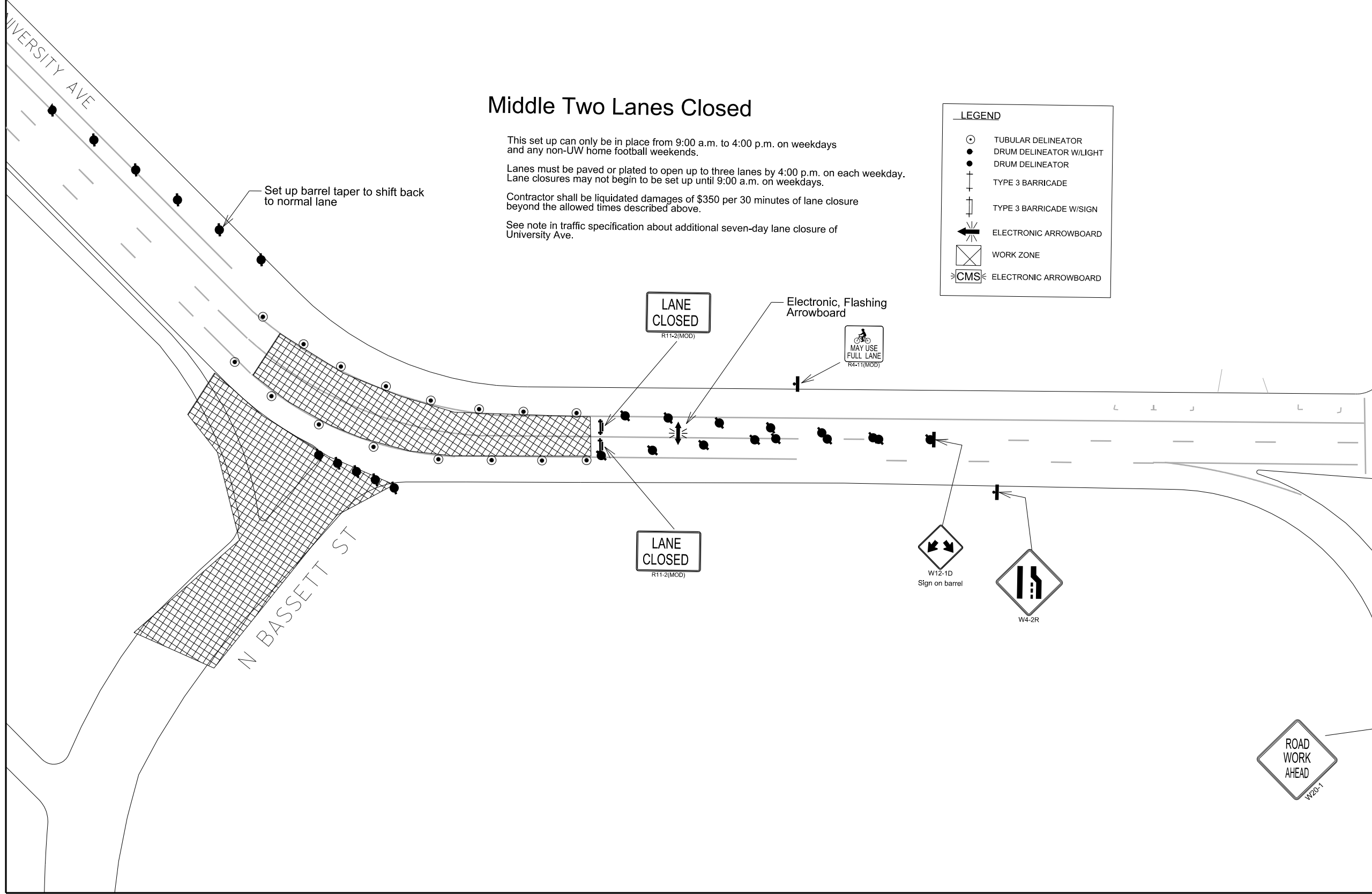
LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	CMS ELECTRONIC ARROWBOARD

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.





LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	ELECTRONIC ARROWBOARD

Right Two Lanes Closed

This set up is only allowed from 9:00 a.m. to 4:00 p.m. on weekdays and any non-UW home football weekends.

Traffic control shall not begin to be set up until after 9:00 a.m on weekdays.

Three lanes shall be open at all other times.

Contractor shall be liquidated damages of \$350 per 30 minutes of lane closure beyond the allowed times described above.

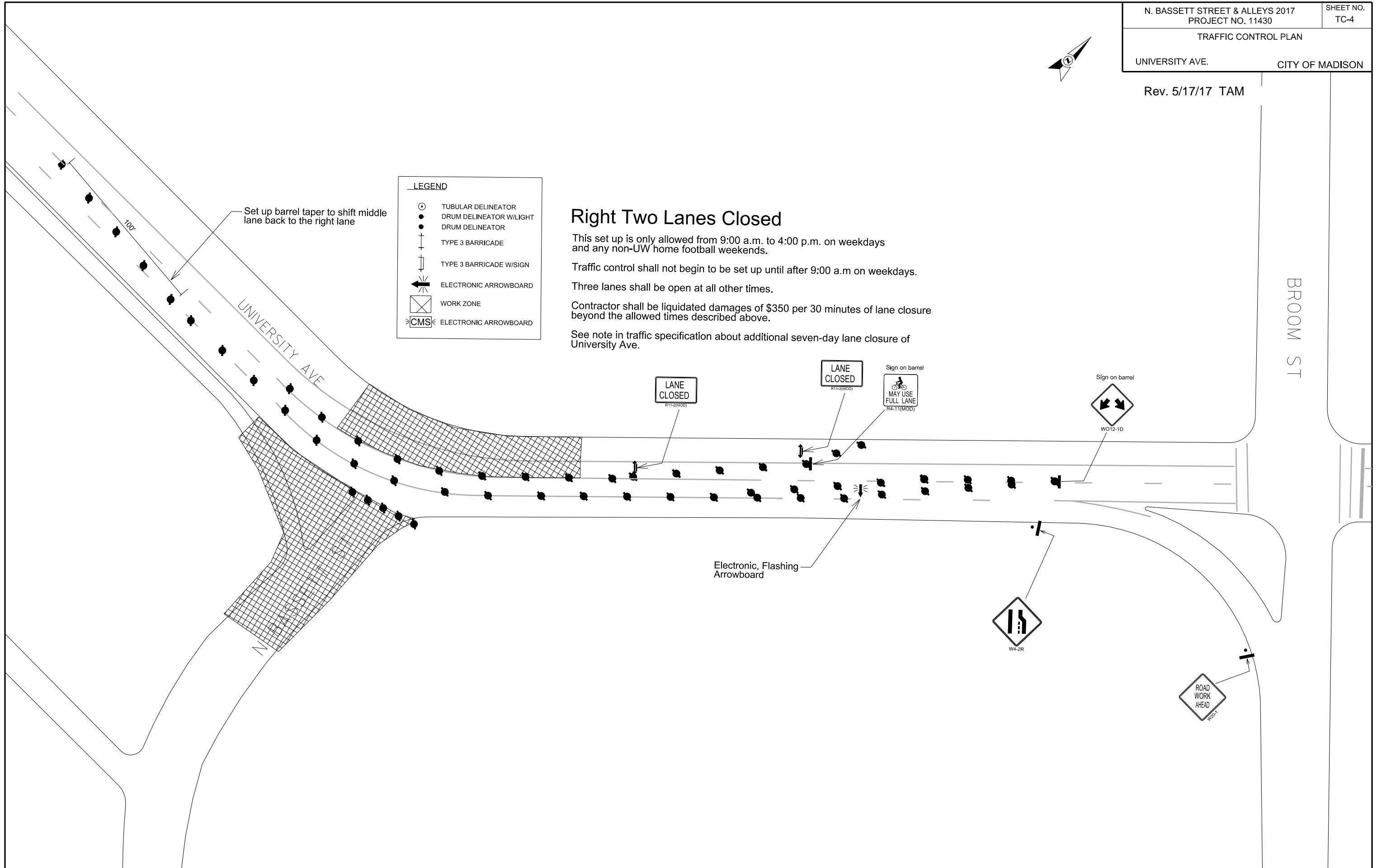
See note in traffic specification about additional seven-day lane closure of University Ave.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.





Left Thru Lane Closure

This set-up may be used at any time.
Three lanes are maintained through the construction zone.

Traffic control set-up cannot actively take place between 7:00 a.m. and 9:00 a.m.
or 3:00 p.m. and 6:00 p.m.

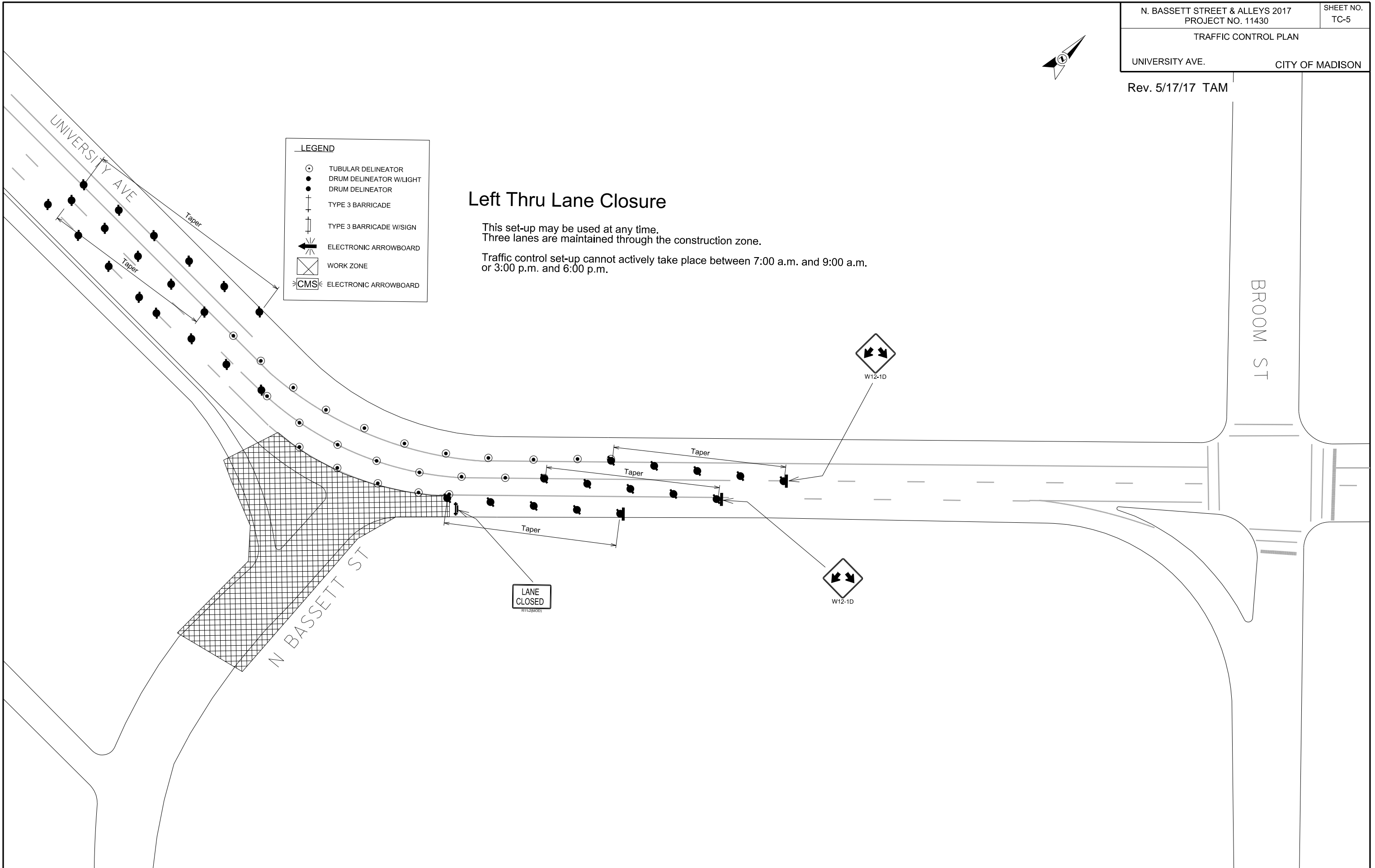
LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	ELECTRONIC ARROWBOARD

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



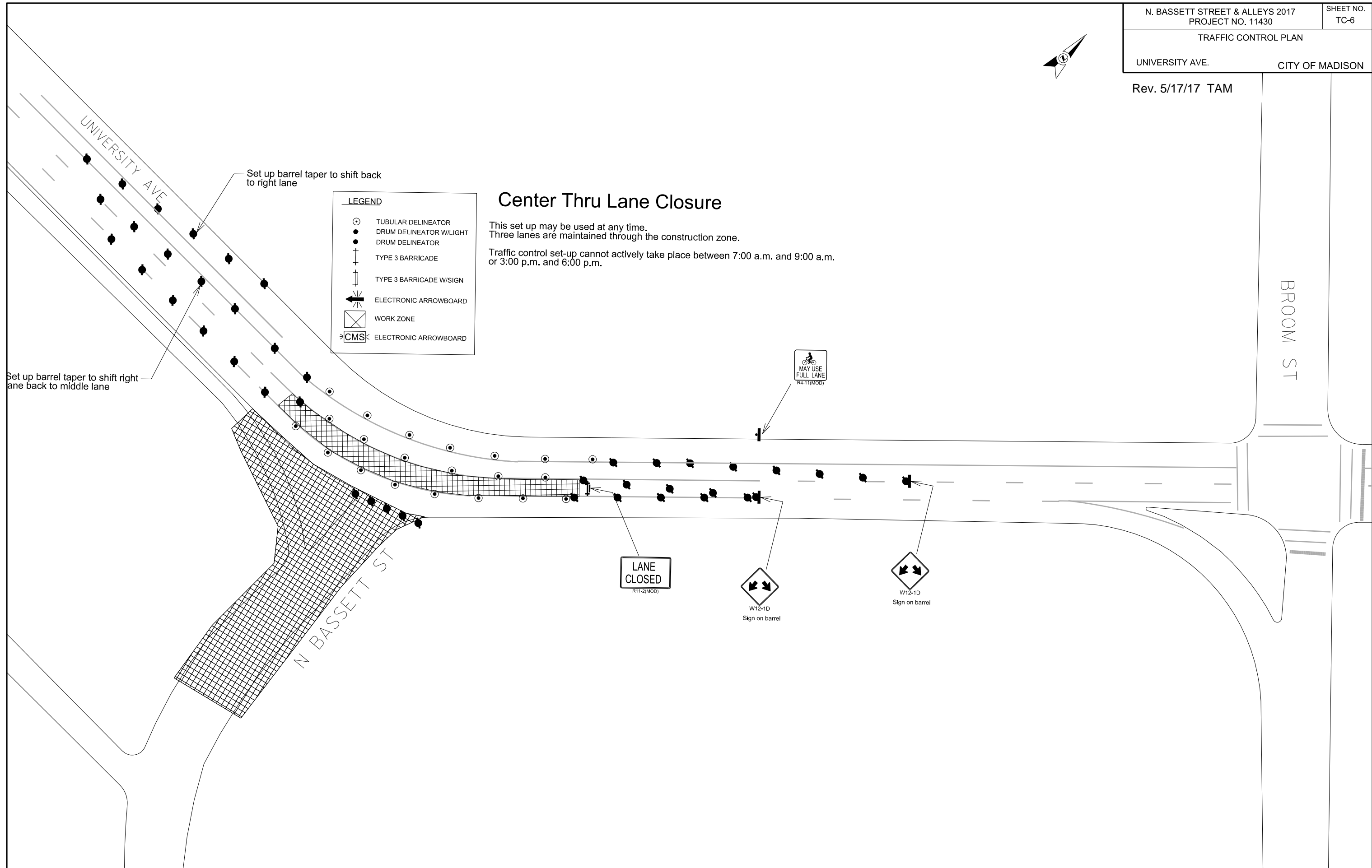


Center Thru Lane Closure

This set up may be used at any time.
Three lanes are maintained through the construction zone.

Traffic control set-up cannot actively take place between 7:00 a.m. and 9:00 a.m.
or 3:00 p.m. and 6:00 p.m.

LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	CMS ELECTRONIC ARROWBOARD



PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

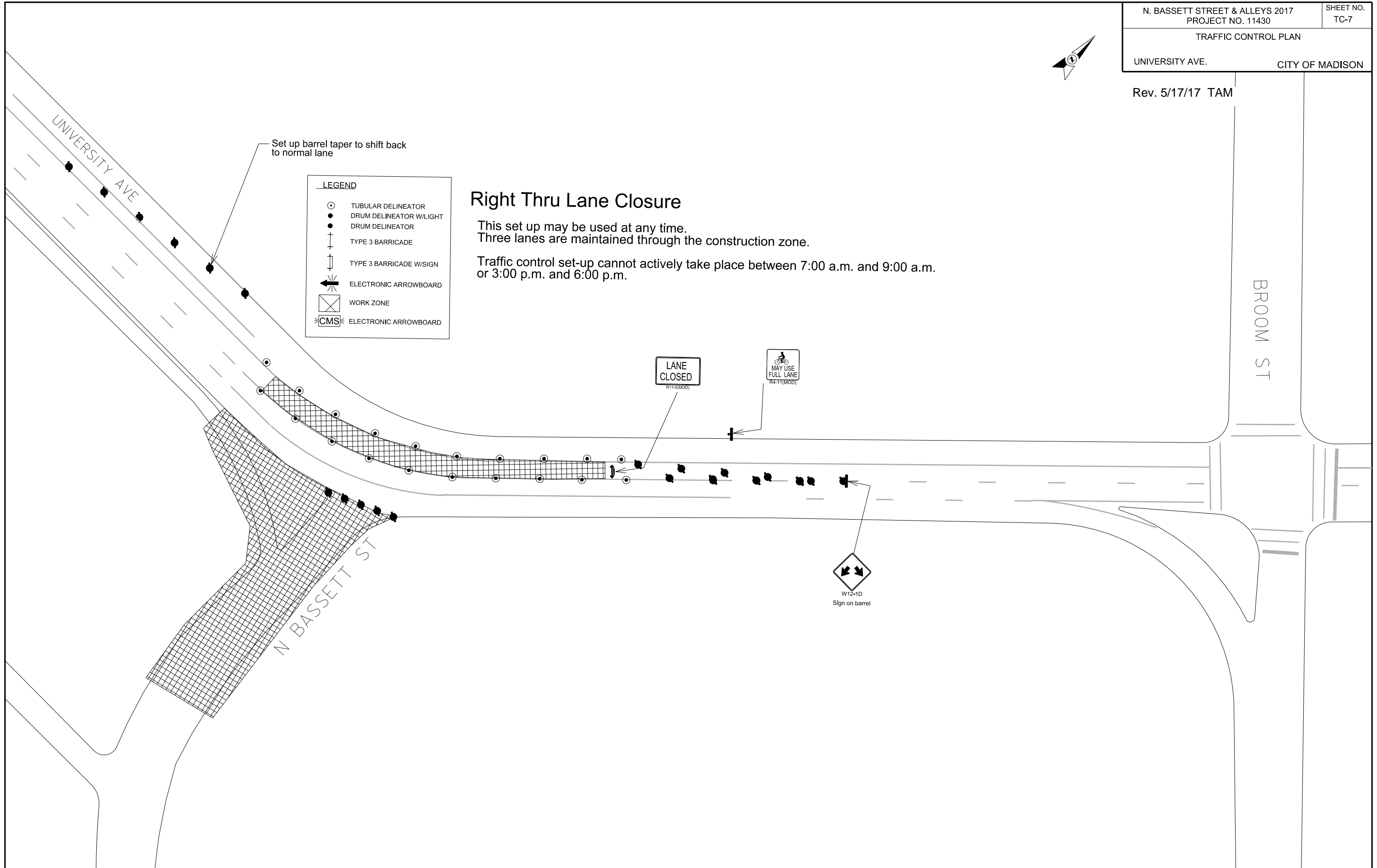


PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



LEGEND

- TUBULAR DELINEATOR
- DRUM DELINEATOR W/LIGHT
- DRUM DELINEATOR
- TYPE 3 BARRICADE
- TYPE 3 BARRICADE W/SIGN
- ELECTRONIC ARROWBOARD
- WORK ZONE
- ELECTRONIC ARROWBOARD

Right Thru Lane Closure

This set up may be used at any time.
Three lanes are maintained through the construction zone.

Traffic control set-up cannot actively take place between 7:00 a.m. and 9:00 a.m.
or 3:00 p.m. and 6:00 p.m.

Set up barrel taper to shift back to normal lane

LANE CLOSED
R11-2(MOD)

MAY USE FULL LANE
R4-11(MOD)

W12-1D
Sign on barrel

Rev. 5/17/17 TAM



PLOT SCALE:

PLOT NAME:

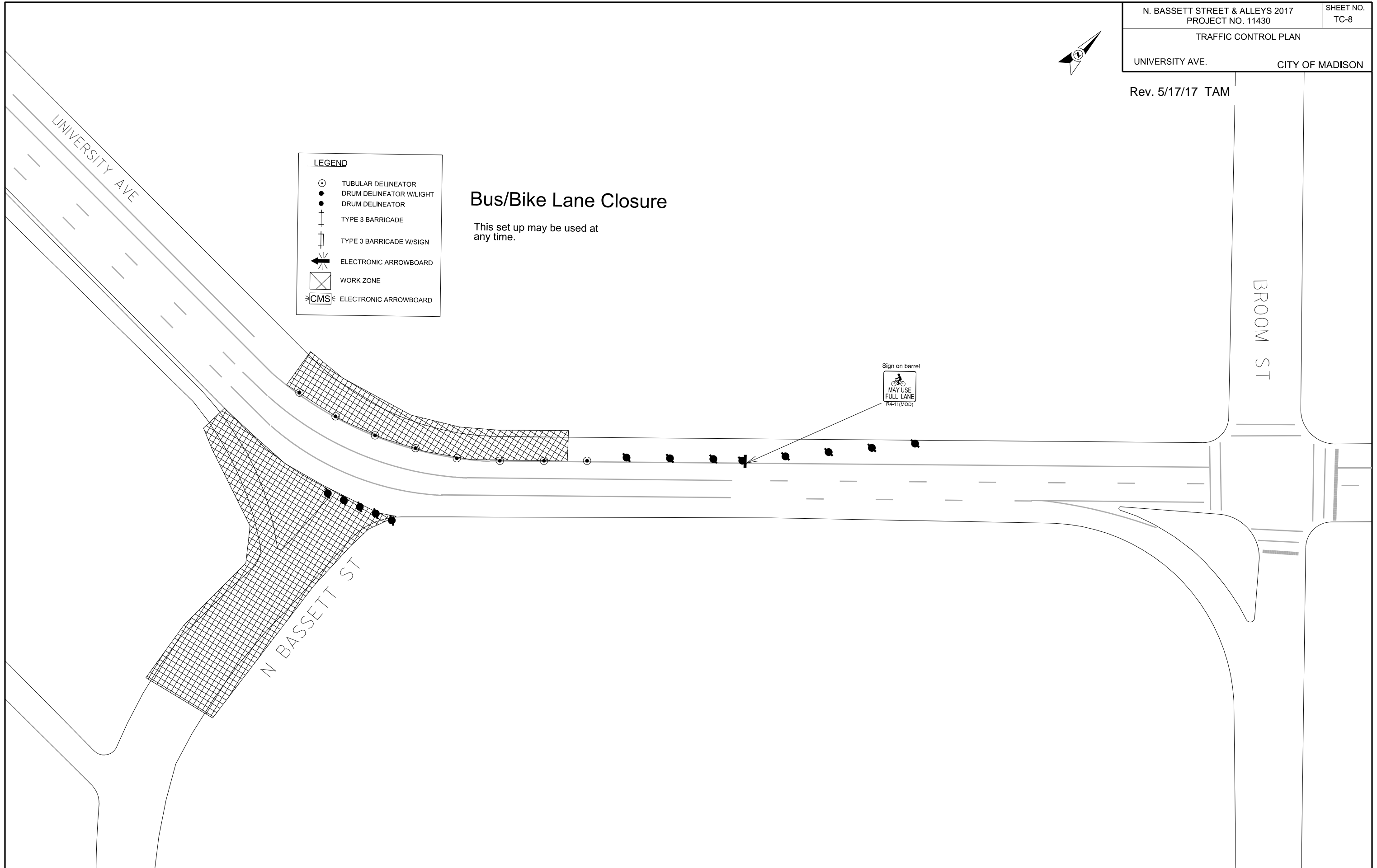
REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	CMS ELECTRONIC ARROWBOARD

Bus/Bike Lane Closure

This set up may be used at any time.



TRAFFIC CONTROL PLAN

W. JOHNSON ST.

CITY OF MADISON

Rev. 5/17/17 TAM

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

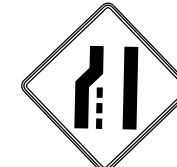
Place sign on new,
temporary post



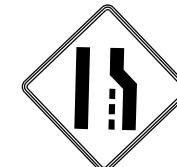
Place sign on new,
temporary post



Place sign on new,
temporary post



or



(based on construction phase)



W JOHNSON ST

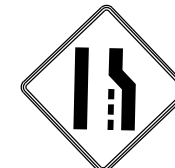
W JOHNSON ST



Place sign on new,
temporary post



Place sign on new,
temporary post



Place sign on new,
temporary post

PARK ST

LAKE ST

FRANCES ST

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

This set up is allowed at all times.

N FRANCES ST

MARION ST

W JOHNSON ST

Electronic, Flashing
Arrowboard

LEGEND

- TUBULAR DELINEATOR
- DRUM DELINEATOR W/LIGHT
- DRUM DELINEATOR
- ⊥ TYPE 3 BARRICADE
- ⊥ TYPE 3 BARRICADE W/SIGN
- ← ELECTRONIC ARROWBOARD
- ⊠ WORK ZONE
- ⊢ CMS ⊣ ELECTRONIC ARROWBOARD

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

This set up is allowed on weekdays from 7:00 a.m. until 4:00 p.m. Three lanes need to be open at 4:00 p.m.

This set up is allowed at all times on weekends except home UW football Saturdays. On those Saturdays, this set up is allowed until 1:00 p.m. when all three lanes need to be open.

Contractor shall pay liquidated damages of \$350 per 30 minutes of lane closure beyond the allowed times described above.

See note in traffic specification about additional seven-day lane closure of Johnson Street.

PLOT SCALE:

PLOT NAME:

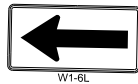
REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

N FRANCES ST

W JOHNSON ST

MARION ST



Electronic, Flashing Arrowboard

LEGEND

- TUBULAR DELINEATOR
- DRUM DELINEATOR W/LIGHT
- DRUM DELINEATOR
- TYPE 3 BARRICADE
- TYPE 3 BARRICADE W/SIGN
- ELECTRONIC ARROWBOARD
- WORK ZONE
- ELECTRONIC ARROWBOARD

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

This set up is only allowed on two separate days.

This set up is allowed on weekdays from 9:00 a.m.
until 3:00 p.m. Three lanes need to be open at 3:00 p.m.

This set up is allowed on weekends from 7:00 a.m.
until 12:00 p.m.

Days that Johnson Street may not be taken down to one lane:
Sunday, July 16 (Ride the Drive)
Any home UW football game.

Contractor shall pay liquidated damages of \$350 per 30 minutes
of lane closure that is extended beyond the allowed times described above.

PLOT SCALE:

PLOT NAME:

REV. DATE:

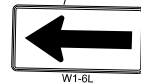
ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

N FRANCES ST

W JOHNSON ST

MARION ST

Electronic, Flashing
Arrowboard



LEGEND

- TUBULAR DELINEATOR
- DRUM DELINEATOR W/LIGHT
- DRUM DELINEATOR
- ⊢ TYPE 3 BARRICADE
- ⊢ TYPE 3 BARRICADE W/SIGN
- ← ELECTRONIC ARROWBOARD
- ⊠ WORK ZONE
- ⊢CMS⊢ ELECTRONIC ARROWBOARD

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

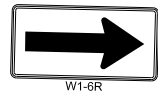
This set up is only allowed on two separate days.

This set up is allowed on weekdays from 9:00 a.m. until 3:00 p.m. Three lanes need to be open at 3:00 p.m.

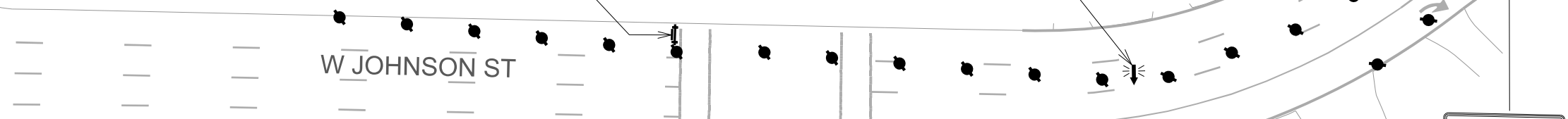
This set up is allowed on weekends from 7:00 a.m. until 12:00 p.m.

Days that Johnson Street may not be taken down to one lane:
Sunday, July 16 (Ride the Drive)
Any home UW football game.

Contractor shall pay liquidated damages of \$350 per 30 minutes that lane closures extend beyond the allowed times described above.



Electronic, Flashing Arrowboard



LEGEND

	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	ELECTRONIC ARROWBOARD

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

This set up is allowed on weekdays from 7:00 a.m.
until 4:00 p.m. Three lanes need to be open at 4:00 p.m.

This set up is allowed at all times on weekends except
home UW football Saturdays. On those Saturdays, this set up is
allowed until 1:00 p.m. when all three lanes need to be open.

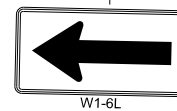
Contractor shall pay liquidated damages of \$350 per 30 minutes
of lane closure beyond the allowed times described above.

See note in traffic specification about additional seven-day lane closure of
Johnson Street.









Electronic, Flashing
Arrowboard

W JOHNSON ST

MARION ST



W1-6L

LEGEND	
	TUBULAR DELINEATOR
	DRUM DELINEATOR W/LIGHT
	DRUM DELINEATOR
	TYPE 3 BARRICADE
	TYPE 3 BARRICADE W/SIGN
	ELECTRONIC ARROWBOARD
	WORK ZONE
	ELECTRONIC ARROWBOARD

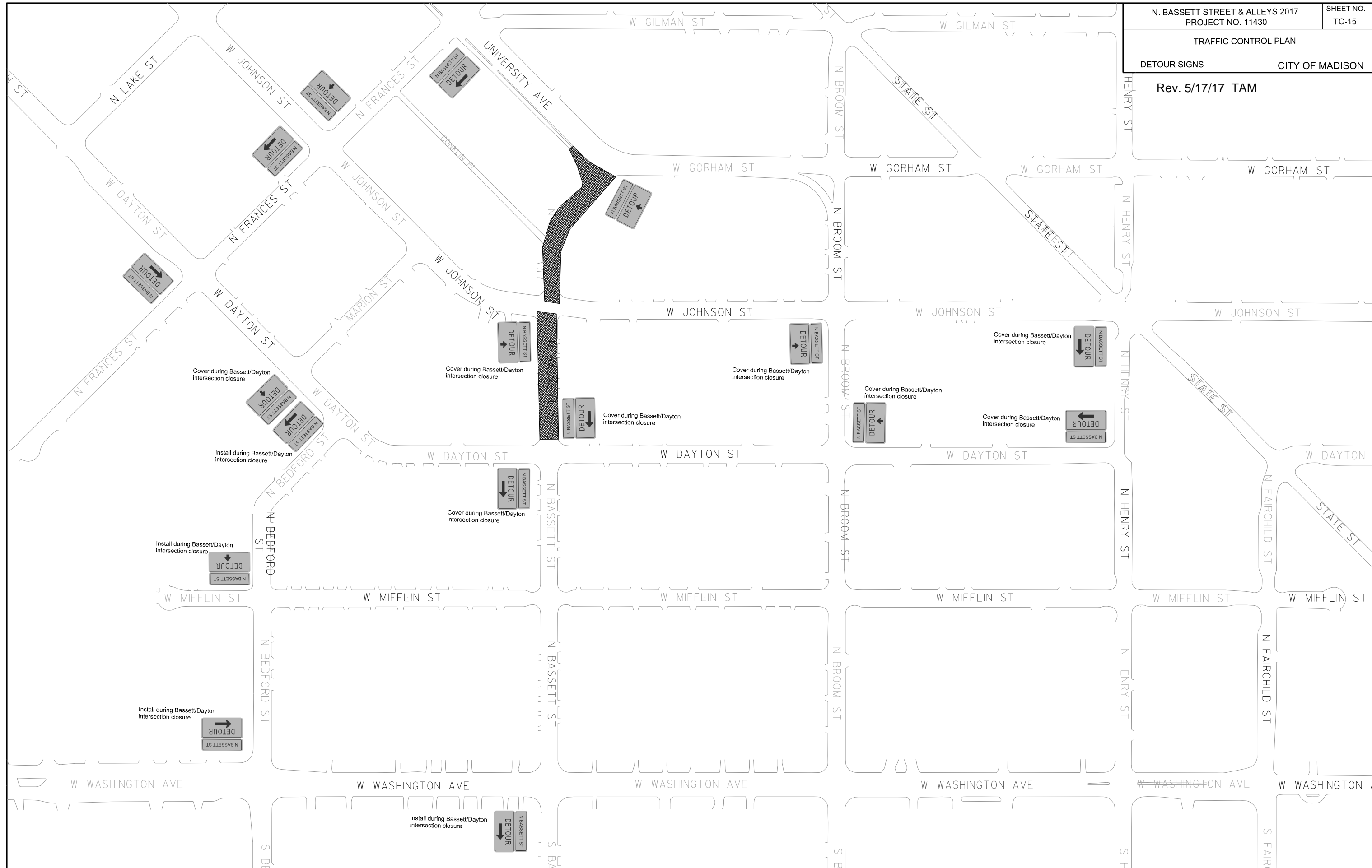
PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

Rev. 5/17/17 TAM



PLOT SCALE:
PLOT NAME:
REV. DATE:
ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

TRAFFIC CONTROL PLAN

W. JOHNSON ST. CITY OF MADISON

Rev. 5/17/17 TAM

This set up is allowed on weekdays from 7:00 a.m.
until 4:00 p.m. Three lanes need to be open at 4:00 p.m.

This set up is allowed at any time on weekends, except home UW
football Saturdays. On those days, three lanes shall be open at 1:00 p.m.

Contractor shall pay liquidated damages of \$350 per 30 minutes
that lane closures extend beyond the allowed times described above.

See note in traffic specification about additional seven-day lane closure of
Johnson Street.

LANE
CLOSED
R11-2(MOD)

Electronic, Flashing
Arrowboard

W JOHNSON ST

N FRANCES ST

MARION ST



LEGEND

- TUBULAR DELINEATOR
- DRUM DELINEATOR W/LIGHT
- DRUM DELINEATOR
- TYPE 3 BARRICADE
- TYPE 3 BARRICADE W/SIGN
- ELECTRONIC ARROWBOARD
- WORK ZONE
- CMS ELECTRONIC ARROWBOARD

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.

CROSS SECTIONS

N. BASSETT ST.

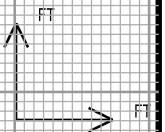
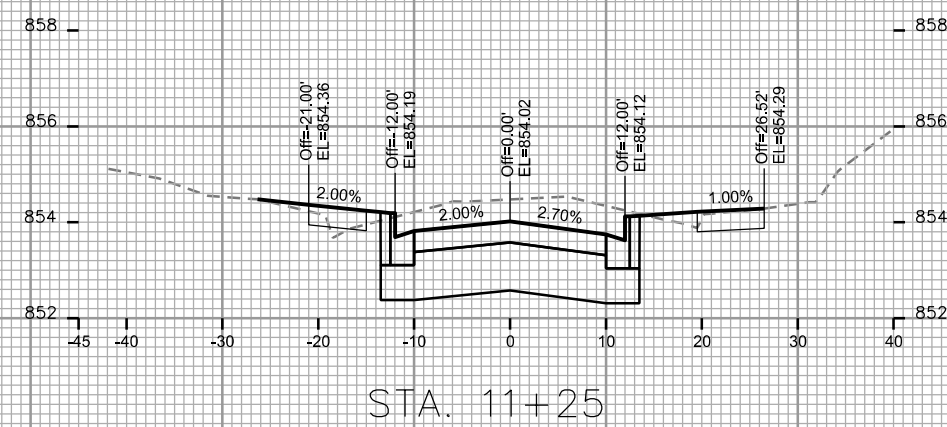
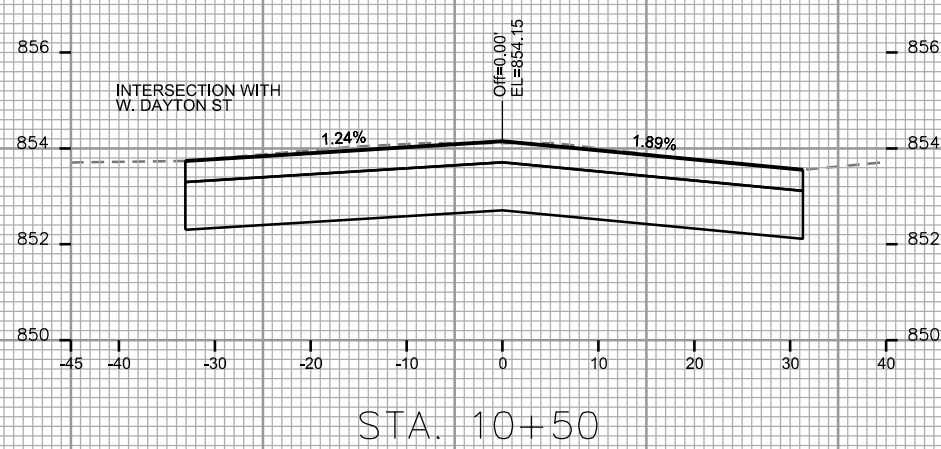
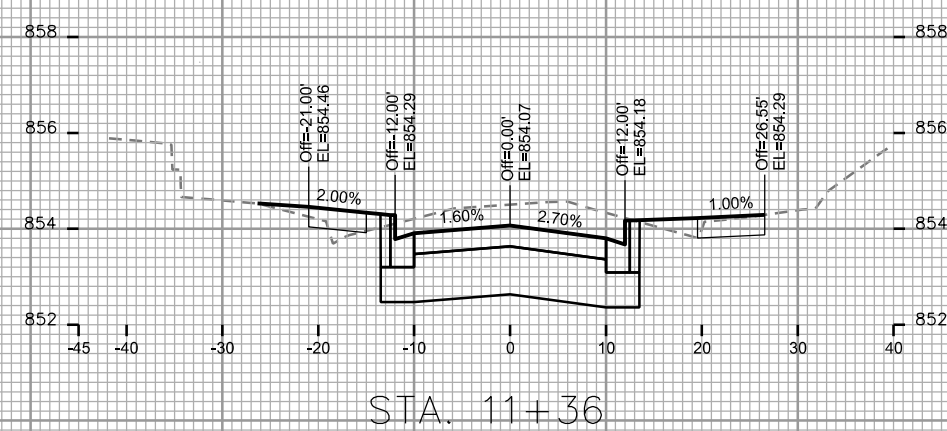
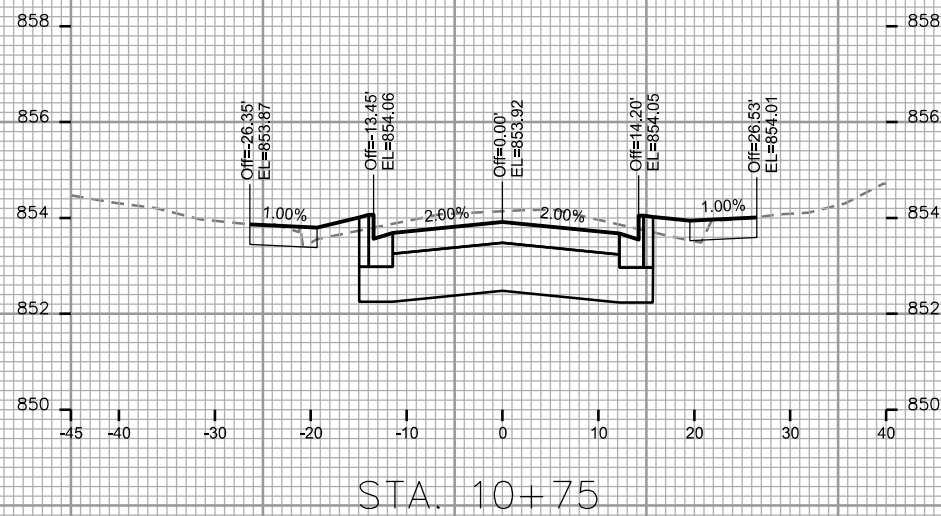
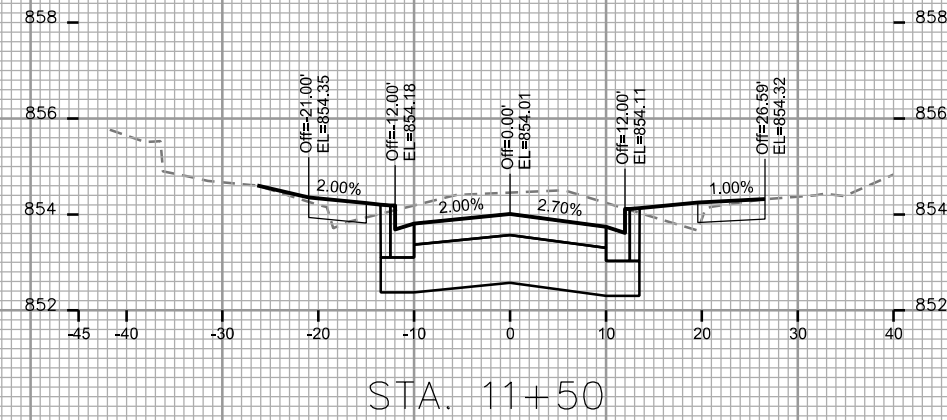
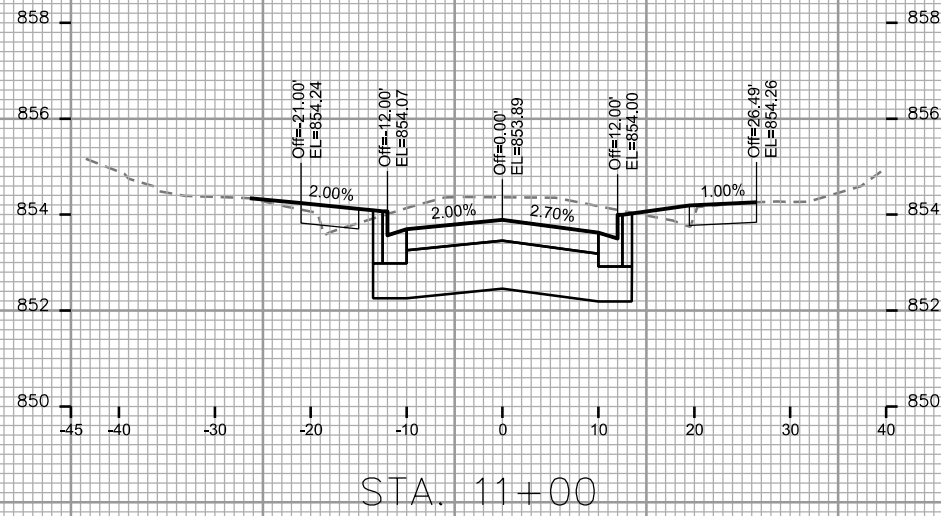
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

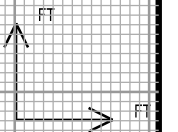
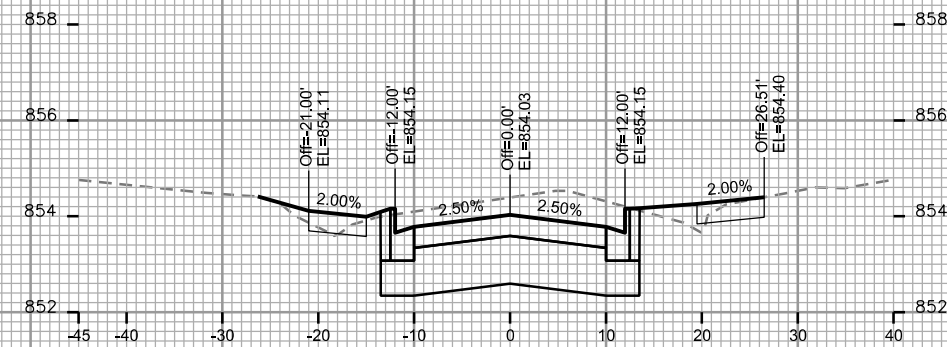
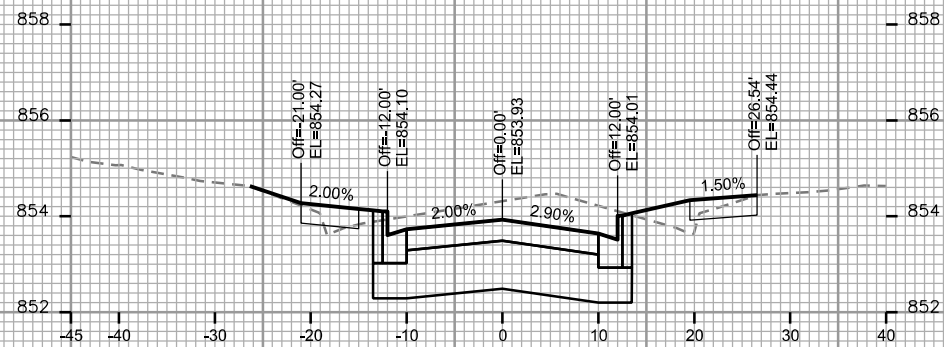
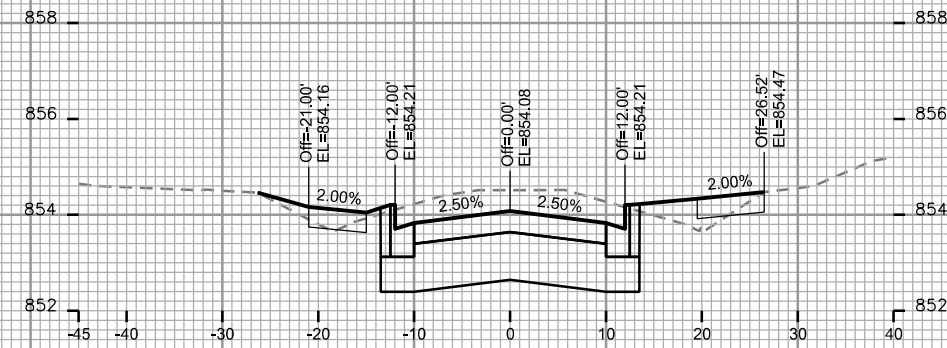
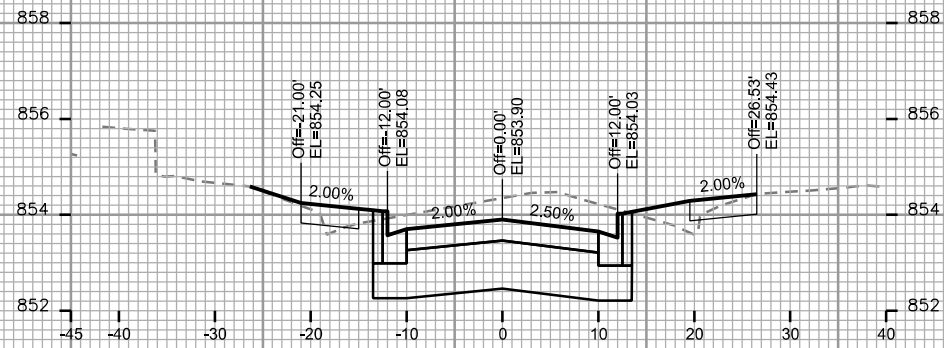
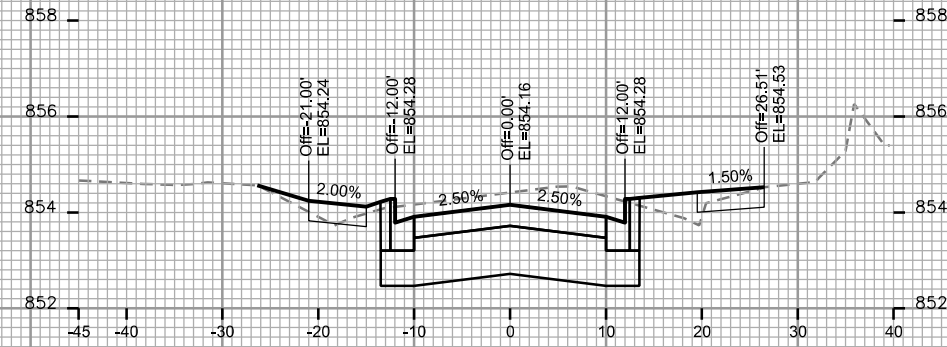
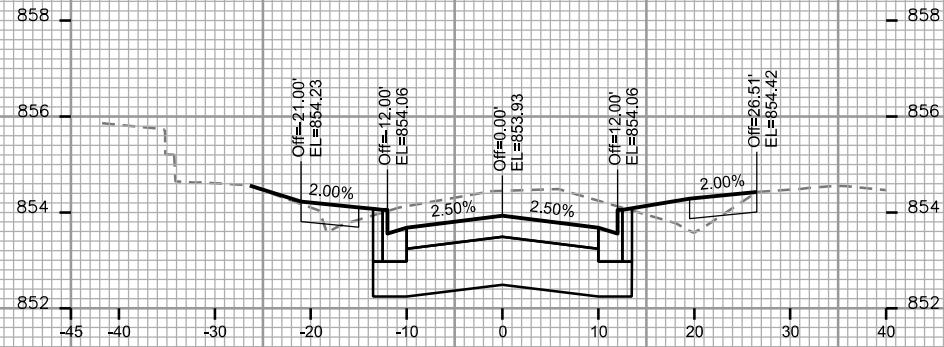
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

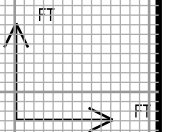
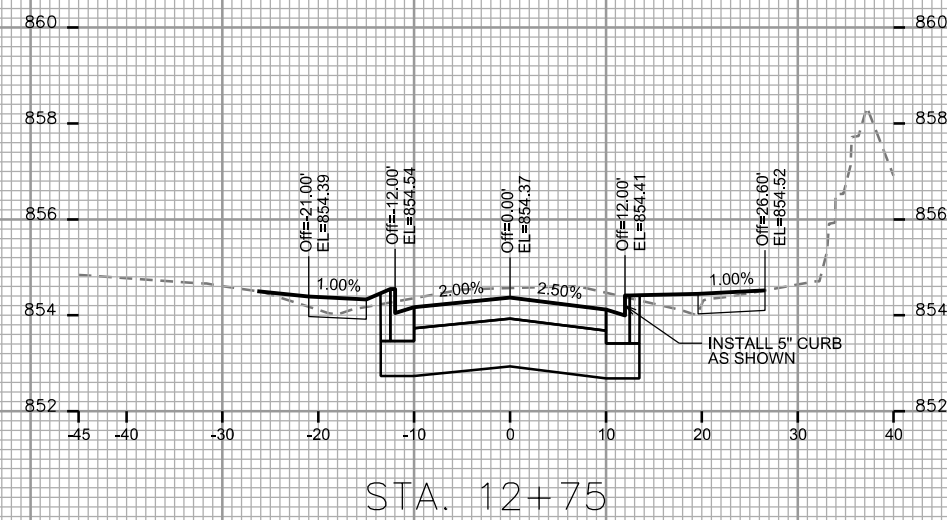
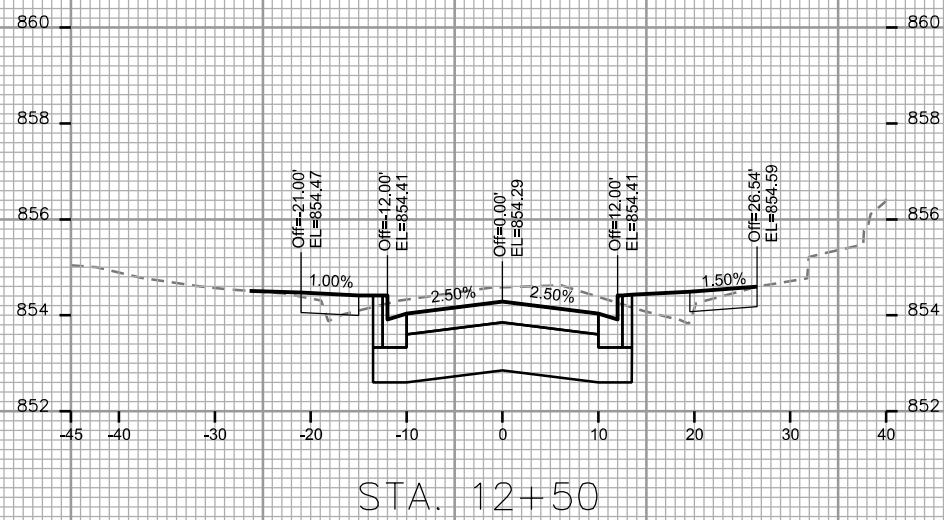
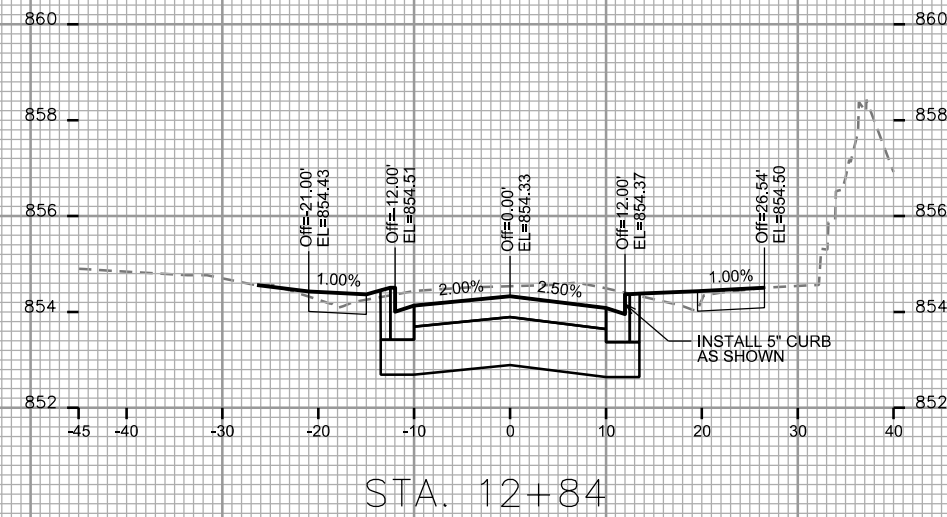
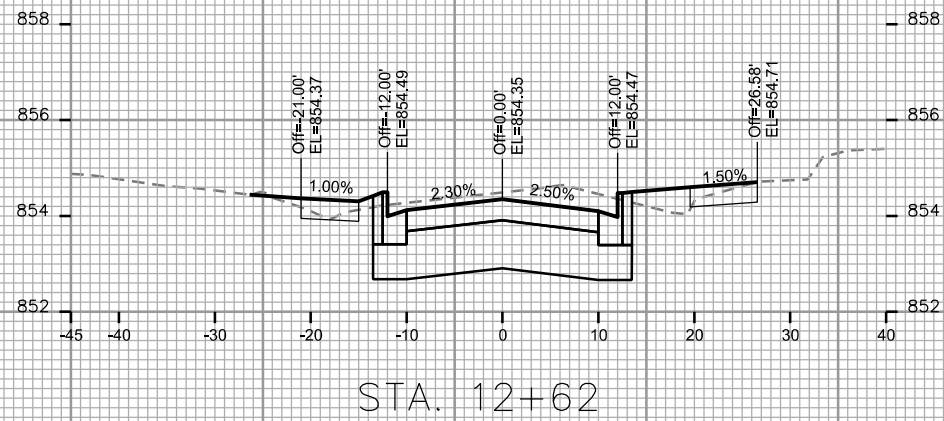
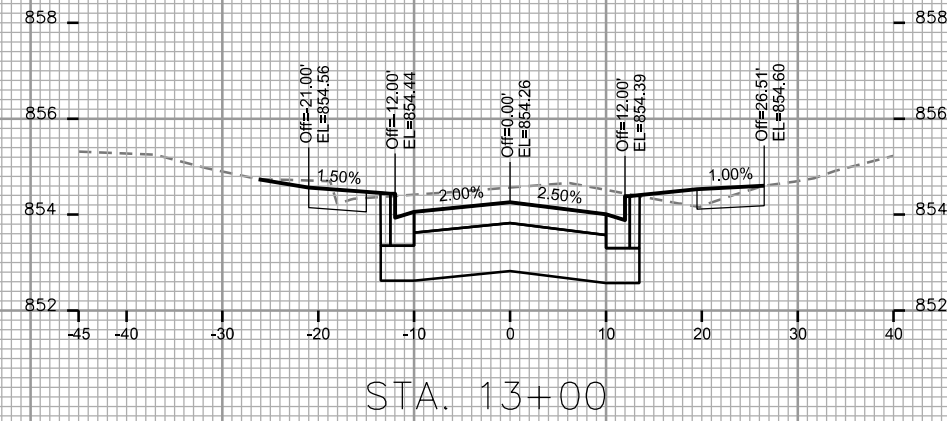
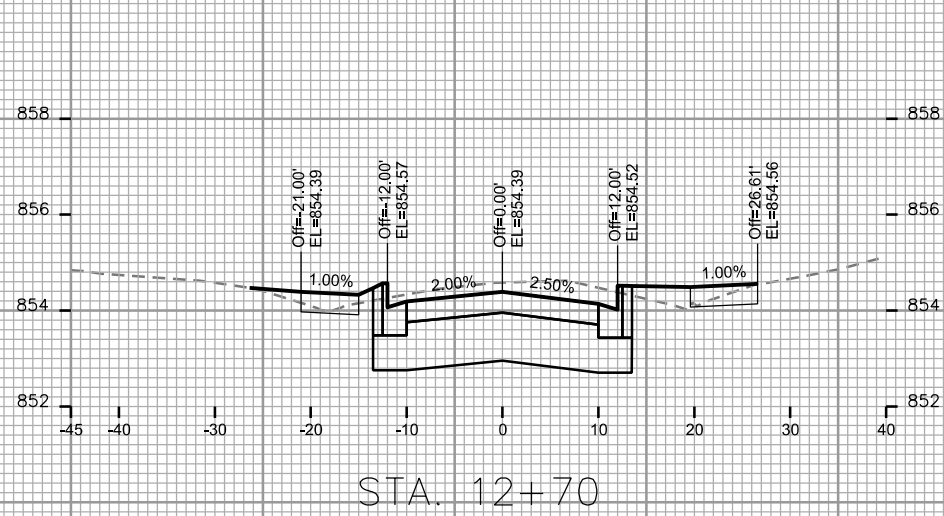
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

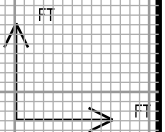
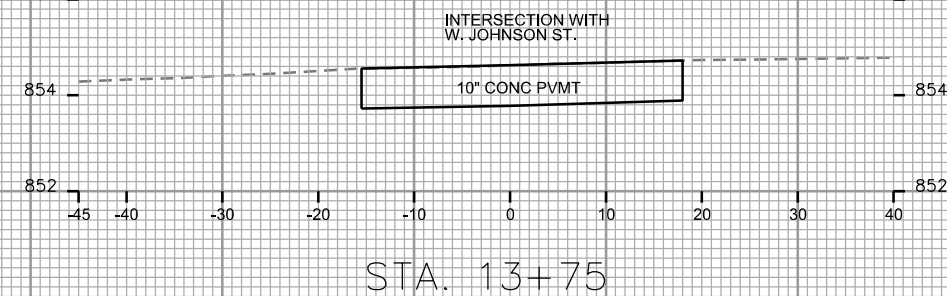
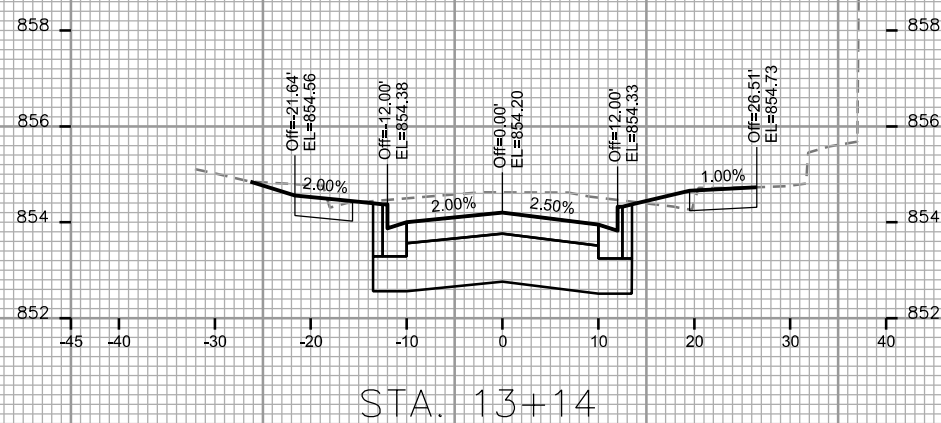
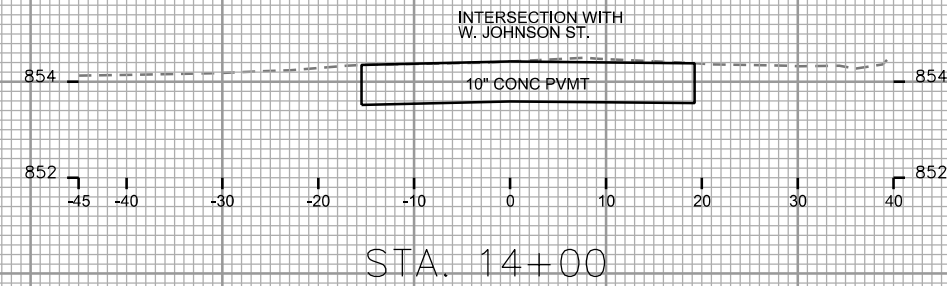
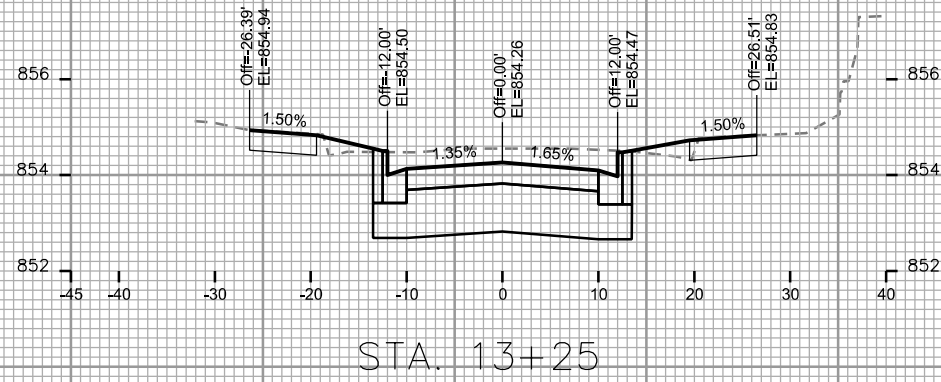
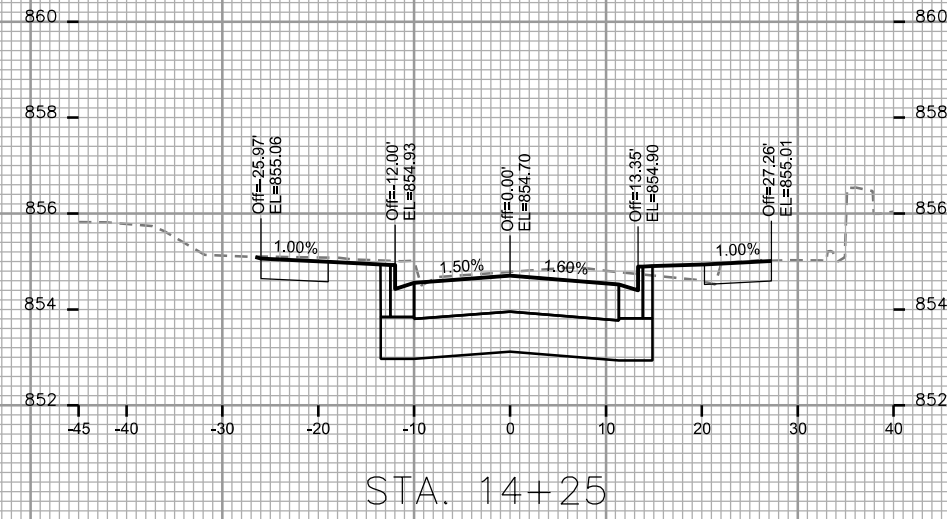
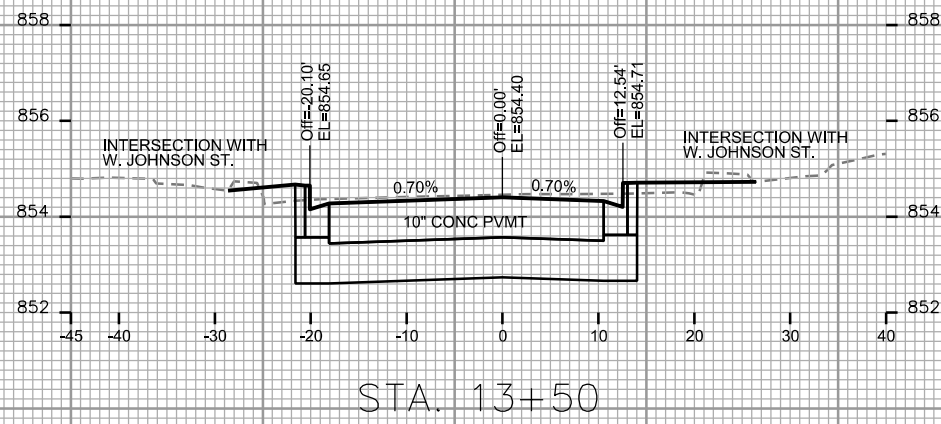
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

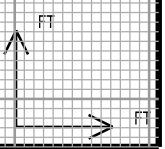
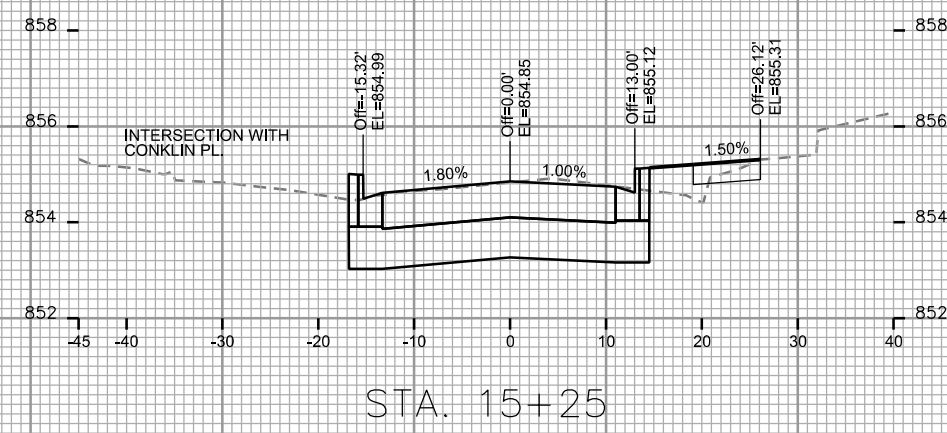
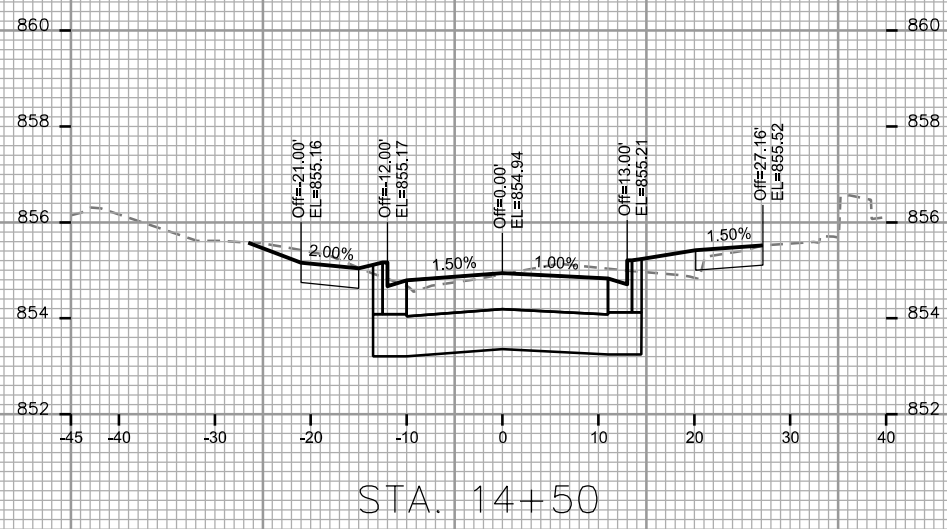
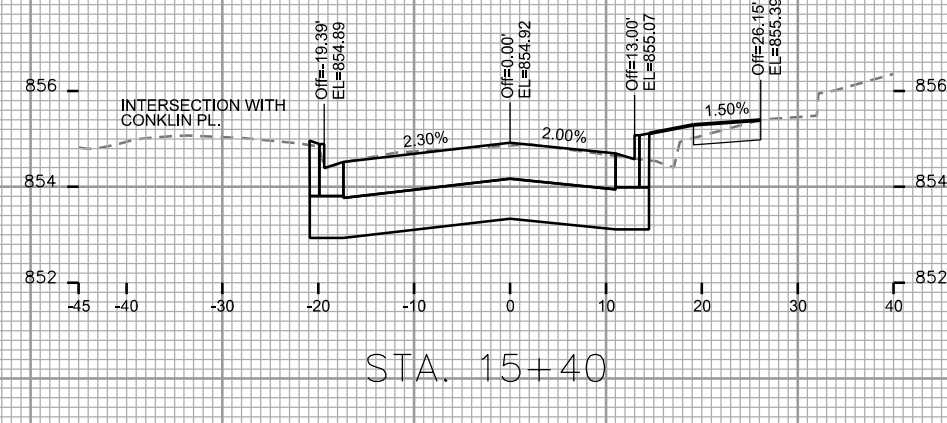
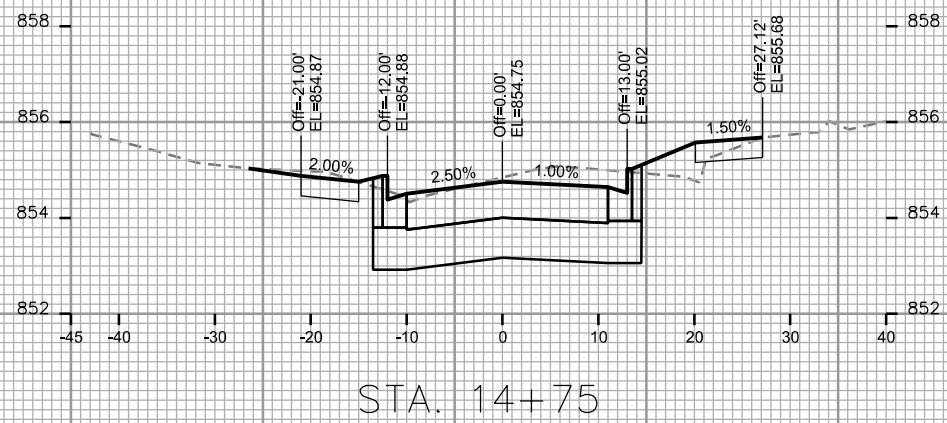
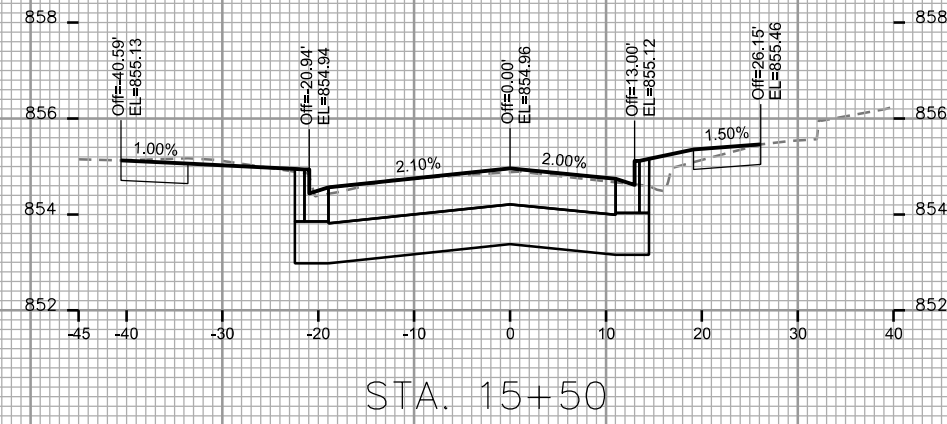
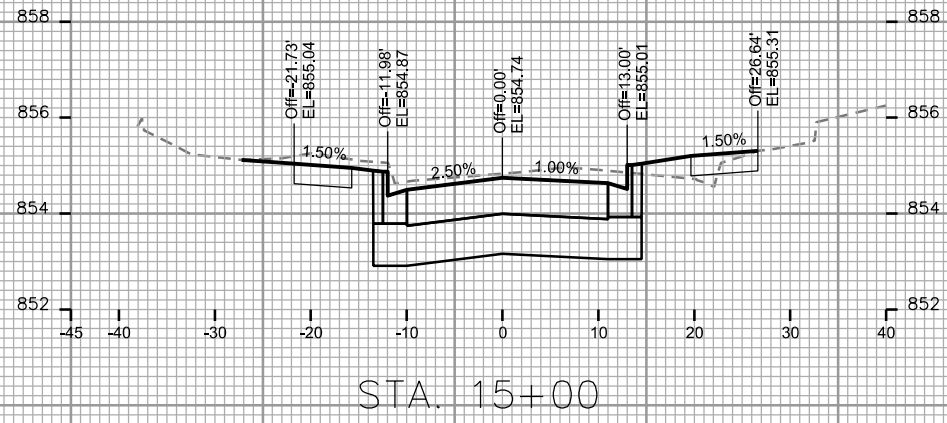
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

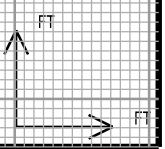
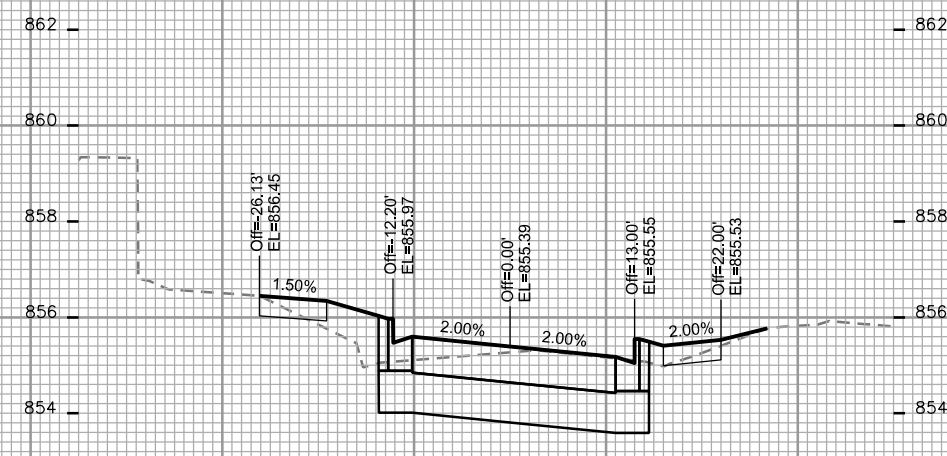
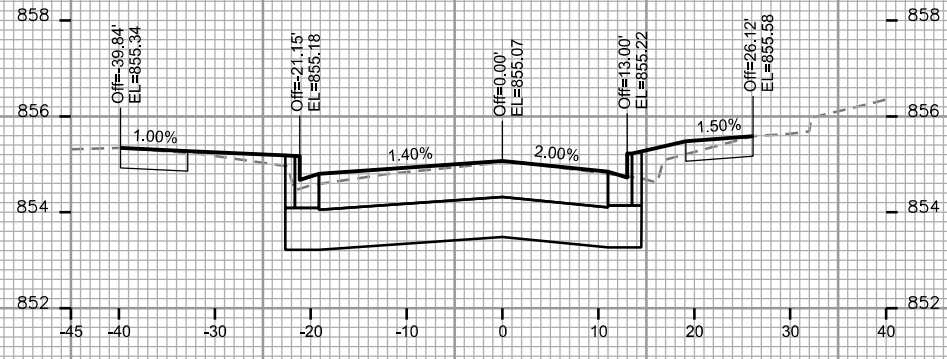
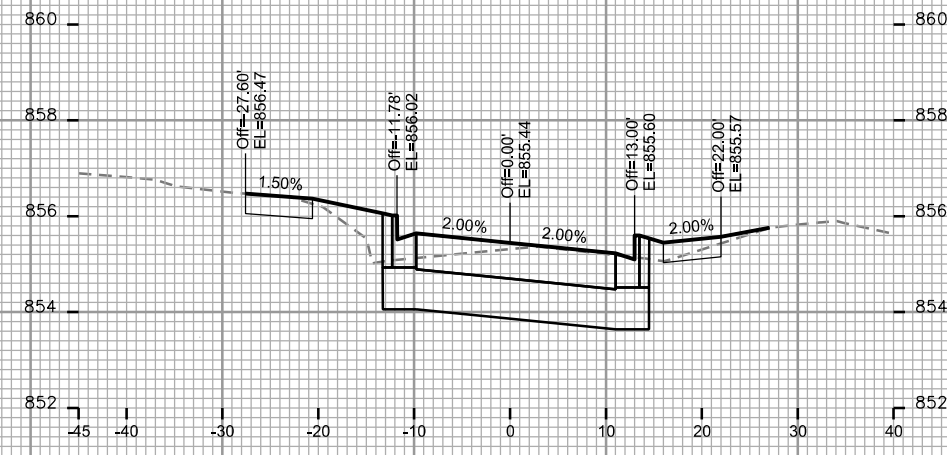
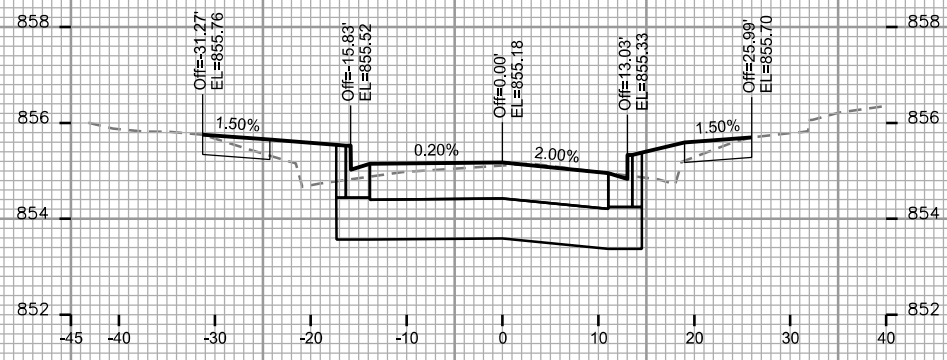
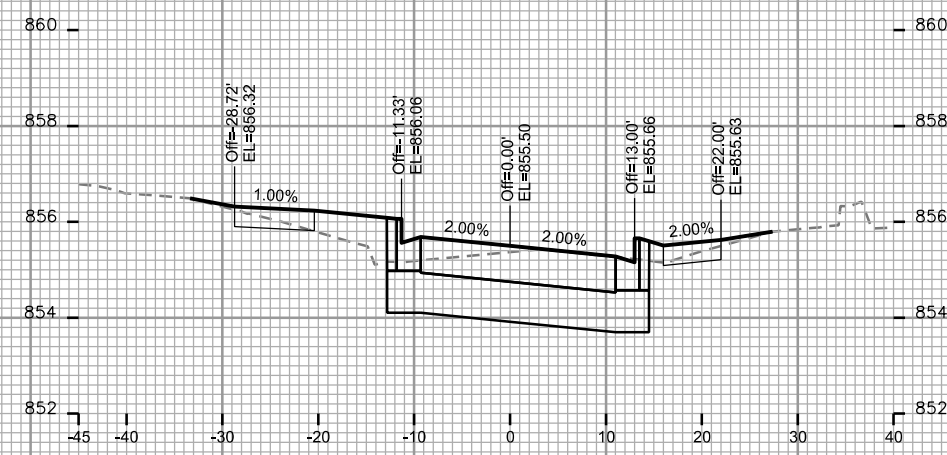
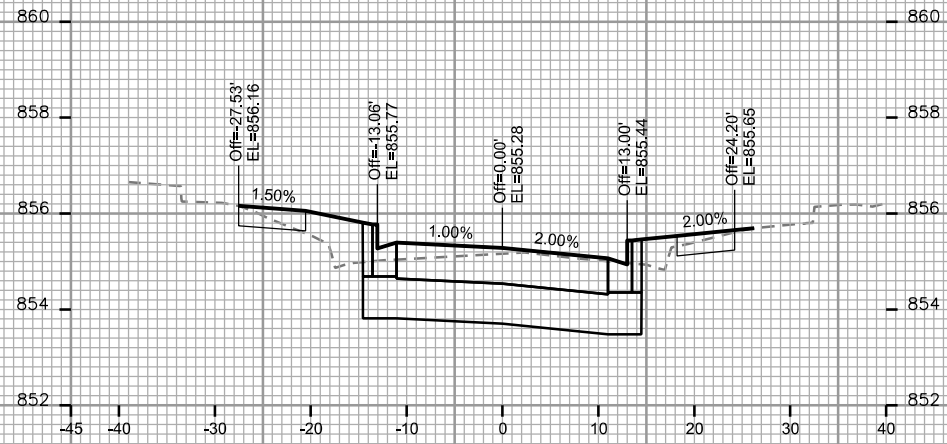
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

N. BASSETT ST.

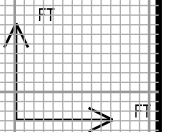
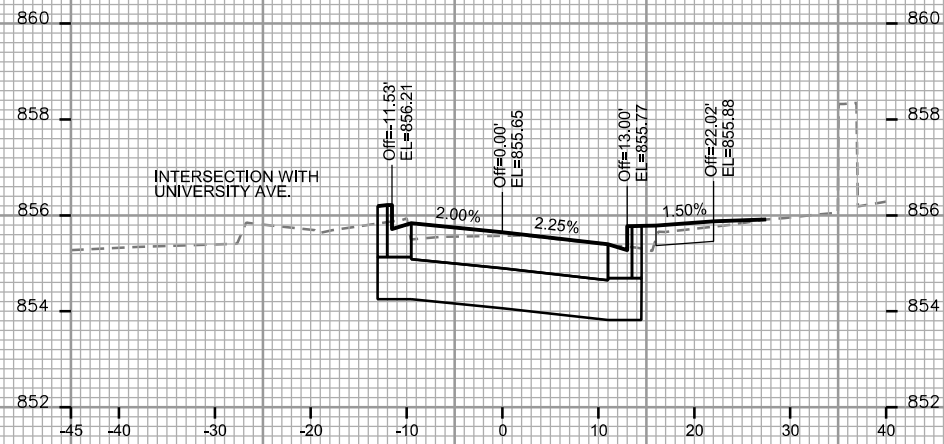
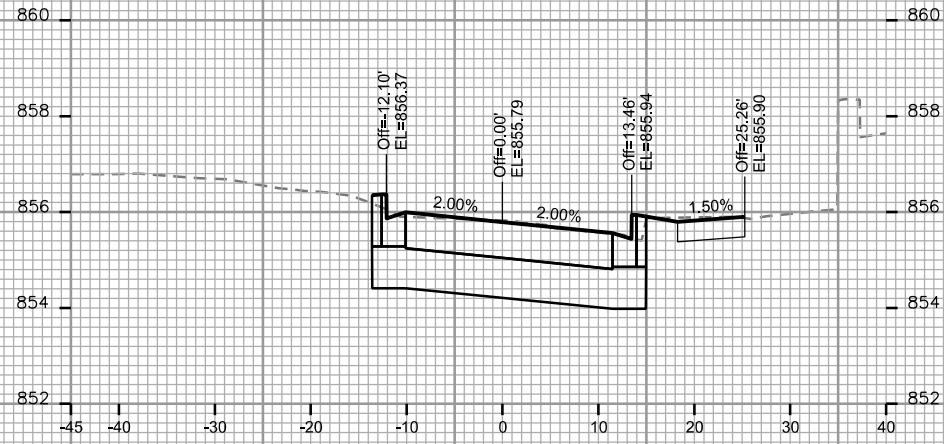
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

CONKLIN ALLEY

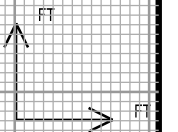
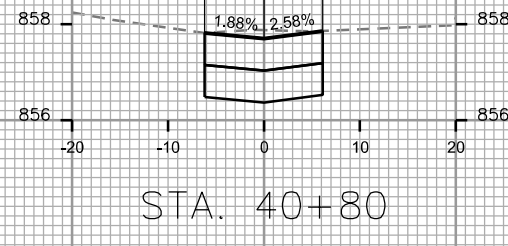
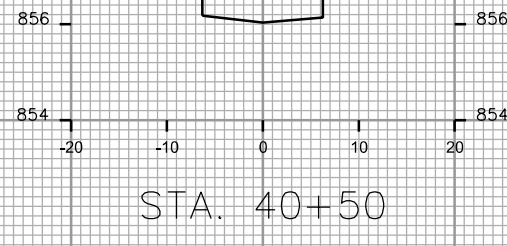
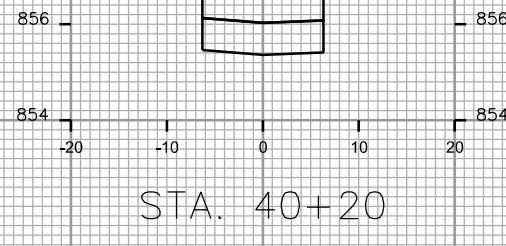
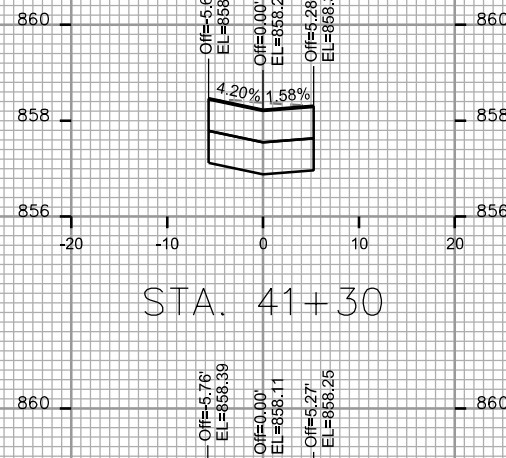
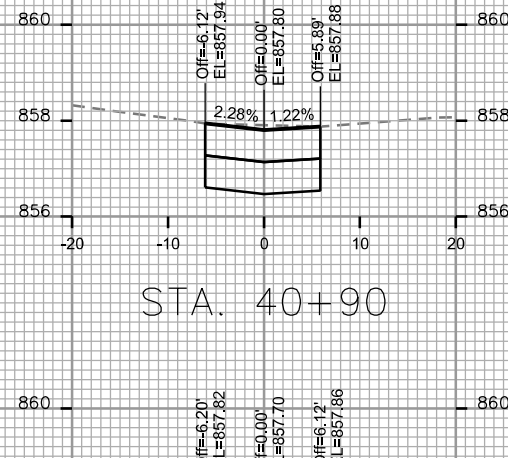
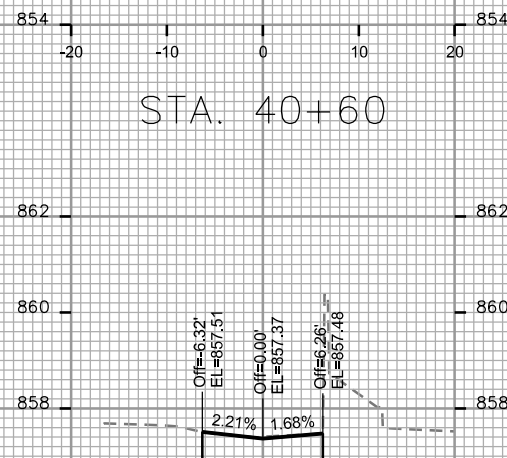
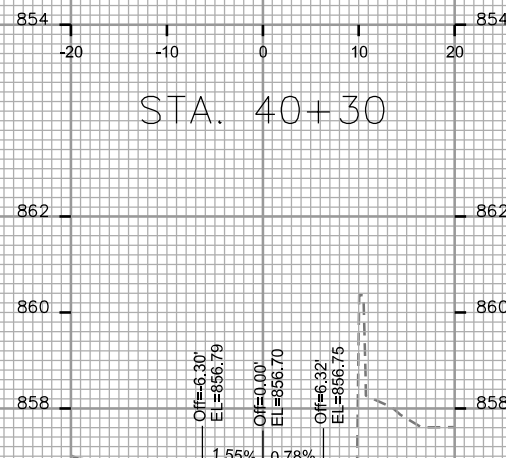
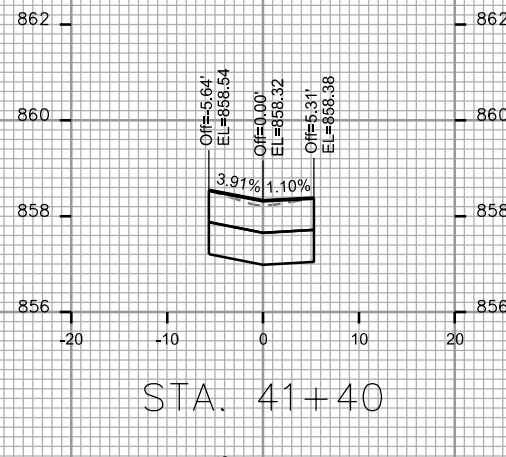
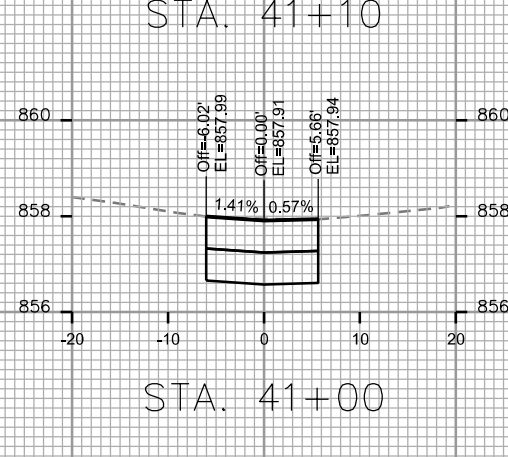
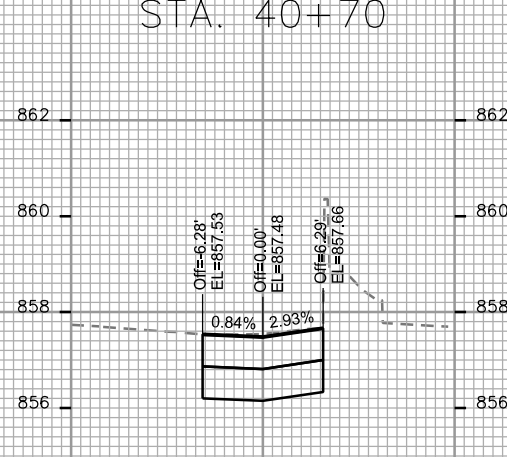
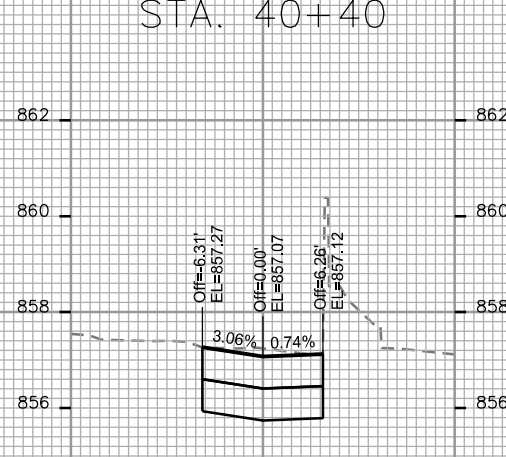
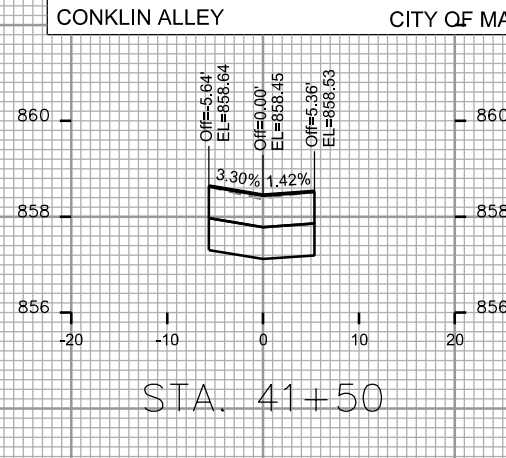
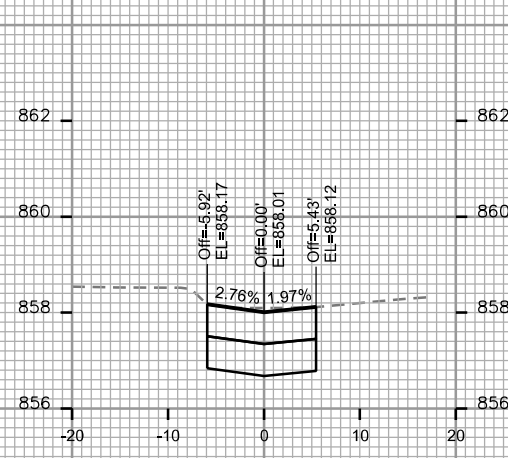
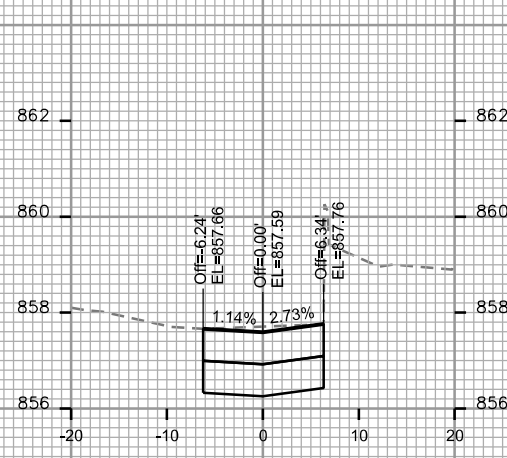
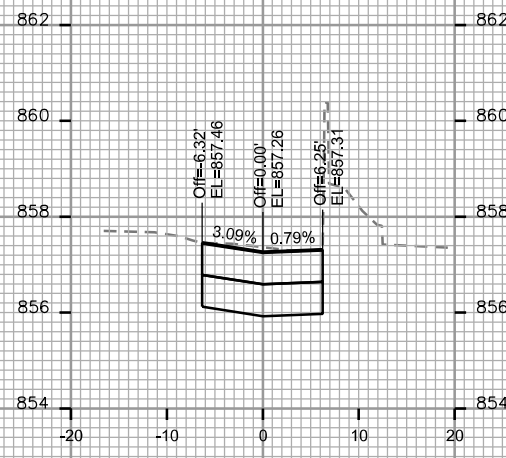
CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

CONKLIN ALLEY

CITY OF MADISON

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

